

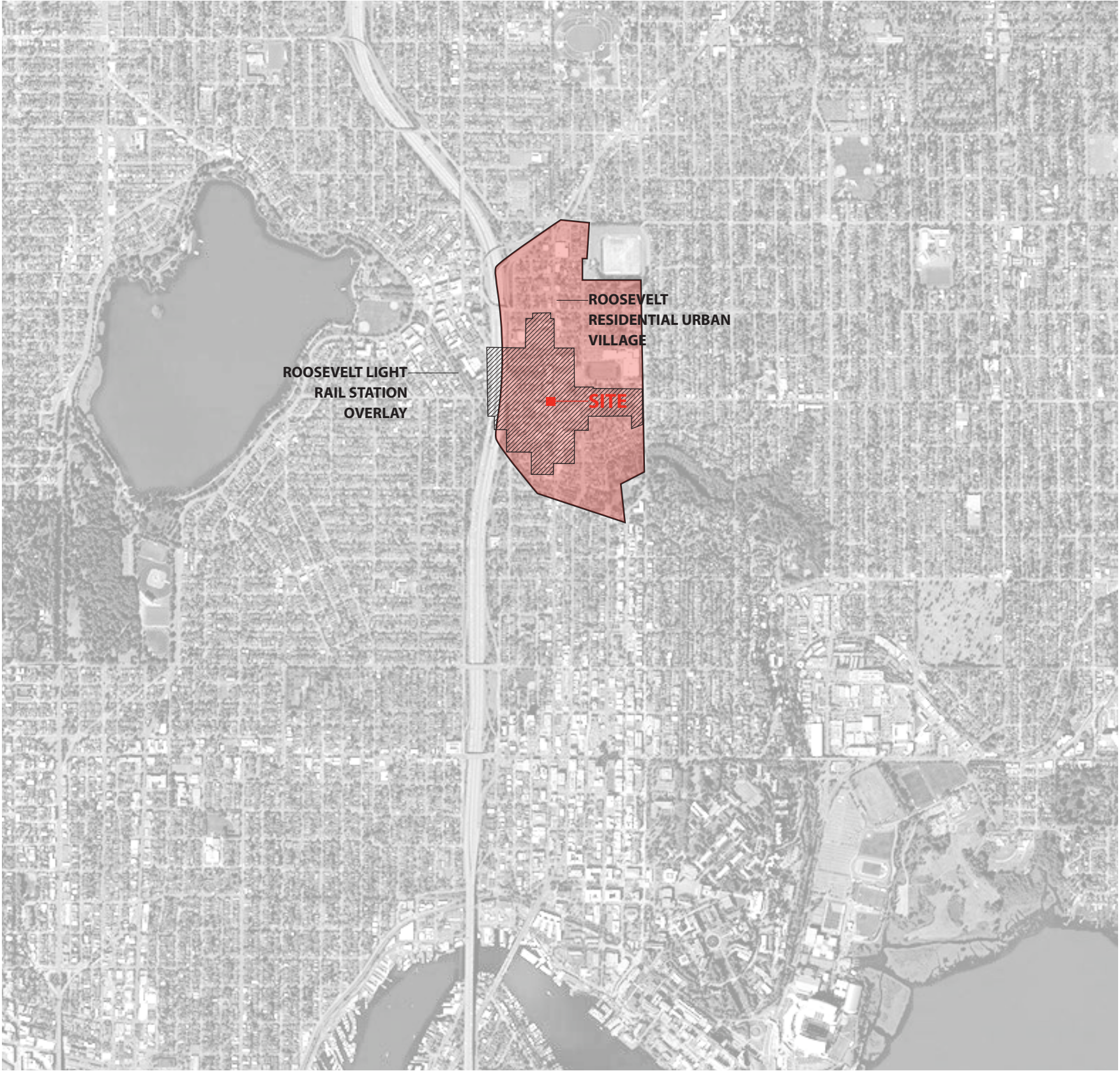
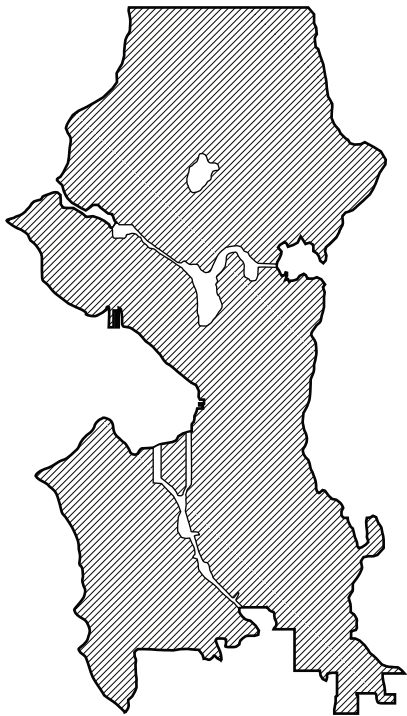
BROOKLYN

EARLY DESIGN GUIDANCE MEETING • May 8, 2017

1309 NE 66th Street • PROJECT NO. 3026788



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Contents

Site Context 1

Development Objectives & Site Context..... 2

Site Context: Land Use..... 3

Site Context: Transit Modes 4

Site Context: Existing Zoning 5

Context: Future Development Zoning Envelopes 6

Context: Zoning Envelope 7

Context: Surrounding Neighborhood..... 8-9

Context: Brooklyn Ave NE 10

Context: NE 66th Street 11

Existing Site Plan & Site Analysis..... 12

Site Plan & Planned Future Developments..... 13

Zoning Data..... 14

Potential Applicable Guidelines 15-17

Design Alternate 1 (No Departures) 18-19

Design Alternate 2 20-21

Design Alternate 3 (Preferred) 22-23

Shadow Study..... 24

Design Alternate Summary 25

Conceptual Landscape Plans..... 26-27

Departure Request Diagrams..... 28-30

Representative Projects 31

Project Information

Property Address:	1309 NE 66th St.
Owner:	Roosevelt Development Group LLC
Developer:	Roosevelt Development Group LLC Nick Miller (Contact) T (206) 812-8126
Architect:	Weinstein A+U LLC Heather Hargesheimer (Contact) T (206) 443-8606
Landscape:	Karen Kiest Landscape Architects

DEVELOPMENT OBJECTIVES & SITE CONTEXT

Development Objectives
 The proposed project is a seven-story apartment building approximately 65-feet in height and approximately 52,800-sf. The building will contain 71 residential units, resident lobby and amenity spaces, a roof deck, 38 structured parking stalls, and a storage and service basement.

- The project development objectives are as follows:
- Provide a high quality living environment for residents with access to public transportation, retail, restaurants, and outdoor amenities;
 - Provide a pedestrian-oriented streetfront on NE 66th St with features that will benefit the neighborhood:
 - Widen the sidewalk and plant street trees to buffer the busy street
 - Strengthen the street edge with an appropriately scaled facade
 - Provide a resident-oriented streetfront on Brooklyn Ave NE:
 - Locate entry to provide eyes on the street
 - Provide landscape to blend with neighbors
 - Provide safe and attractive parking access
 - Locate trash & recycling storage out of sight

- Future Link Light Rail Station
- 1. Roosevelt High School
- 2. Rising Sun Produce
- 3. Pies and Pints
- 4. Whole Foods
- 5. Pizza Hut
- 6. Silhouette Antiques and Gifts
- 7. Transit Oriented Development Sites
- 8. Cowen Park
- 9. Ravenna Park
- 10. Dwell Condominiums
- 11. Bartell Drugs, Wells Fargo, UPS Store
- 12. Toronado Beer Hall
- 13. Health Mutt
- 14. Qwest Communications
- 15. Future 4-Story Mixed-Use Project
- 16. Future 6-story Mixed-Use Project
- 17. Future 7-Story Mixed-Use Project





SITE CONTEXT: LAND USE

NE 68TH ST

The site is bounded by public rights-of-way to the north (NE 66th St) and west (Brooklyn Ave NE). The south and east will be abutted by a proposed 7-story mixed-use building. The current use of the adjacent properties is single family homes.

The neighborhood is in transition due to recent upzoning and incoming Light Rail. The Roosevelt Neighborhood's Core Commercial Area begins west of the site, and is characterized by higher density multi-family housing and retail. The new Roosevelt Light Rail Station will be located one block west of the site along 12th Ave NE.

Directly north of the site is Roosevelt High School, and four blocks south of the project are Cowen and Ravenna Parks.

NE 65TH ST

NE 63RD ST

Legend

Future Light Rail Station (2021)	
Future Development	
Single Family Residential	
Multi Family Residential	
Mixed Use	
Retail	
Office	
Surface Parking	
School / Institutional	
Utility	
Parks/Open Space	
Roosevelt Commercial Core	

SITE CONTEXT: TRANSIT MODES

The project site is well served by several bus lines located within a 5-minute walk radius. King County Metro bus routes run along NE 65th Street:

- 48 - Ballard, Capitol Hill, the Central Area, Mount Baker
- 62 - Hawthorne Hills, Queen Anne, Downtown
- 64 - Jackson Park, UW
- 73 - Jackson Park, UW, Downtown
- 76 - Wedgwood, Hawthorne Hills

The relative flatness of NE 66th Street provides an easy walk to the future light rail station and is slated to become a bicycle route and Green Street. Roosevelt's commercial core is within a 5-minute walk to the west as is Cowen Park. Roosevelt High School is across the street.

Travel times to the University, Capitol Hill and Downtown will greatly improve with the opening of Sound Transit light rail service to Roosevelt Station located just one block to the west.

Legend

Main Car Arterials:

Principal Arterial / Regional Connector

Minor Arterial / Commercial Connector

Future Light Rail Station (2021)

Gateway Intersection per Roosevelt

Frequent Transit Corridors

Pedestrian Overlay

5 Minute Pedestrian Walkshed

Bus Routes

Bus Lines

Current Bicycle Routes

Proposed Bicycle Routes per Seattle Bicycle Master Plan

Proposed Green Street per Roosevelt Streetscape Concept Plan










The map illustrates the site context for transit modes. It shows a grid of streets from 12th Ave NE to 18th Ave NE and NE 63rd St to NE 67th St. The project site is highlighted in red at the intersection of NE 66th St and Brooklyn Ave NE. Key features include: Roosevelt High School to the north; Cowen Park and Ravenna Park to the south; and two 'Gateway' intersections marked with dashed circles. Bus routes are shown as blue lines with route numbers (67, 62, 64, 48, 76, 73, 373, 83, 71). Light rail stations are indicated by red dashed rectangles. Frequent transit corridors are shown as yellow shaded areas. Pedestrian overlays are shown as dotted lines. A 5-minute pedestrian walkshed is shown as a light blue shaded area. Current bicycle routes are shown as green lines, and proposed bicycle routes are shown as dashed green lines. A proposed Green Street is shown as a light green shaded area. A north arrow is located in the bottom left corner.

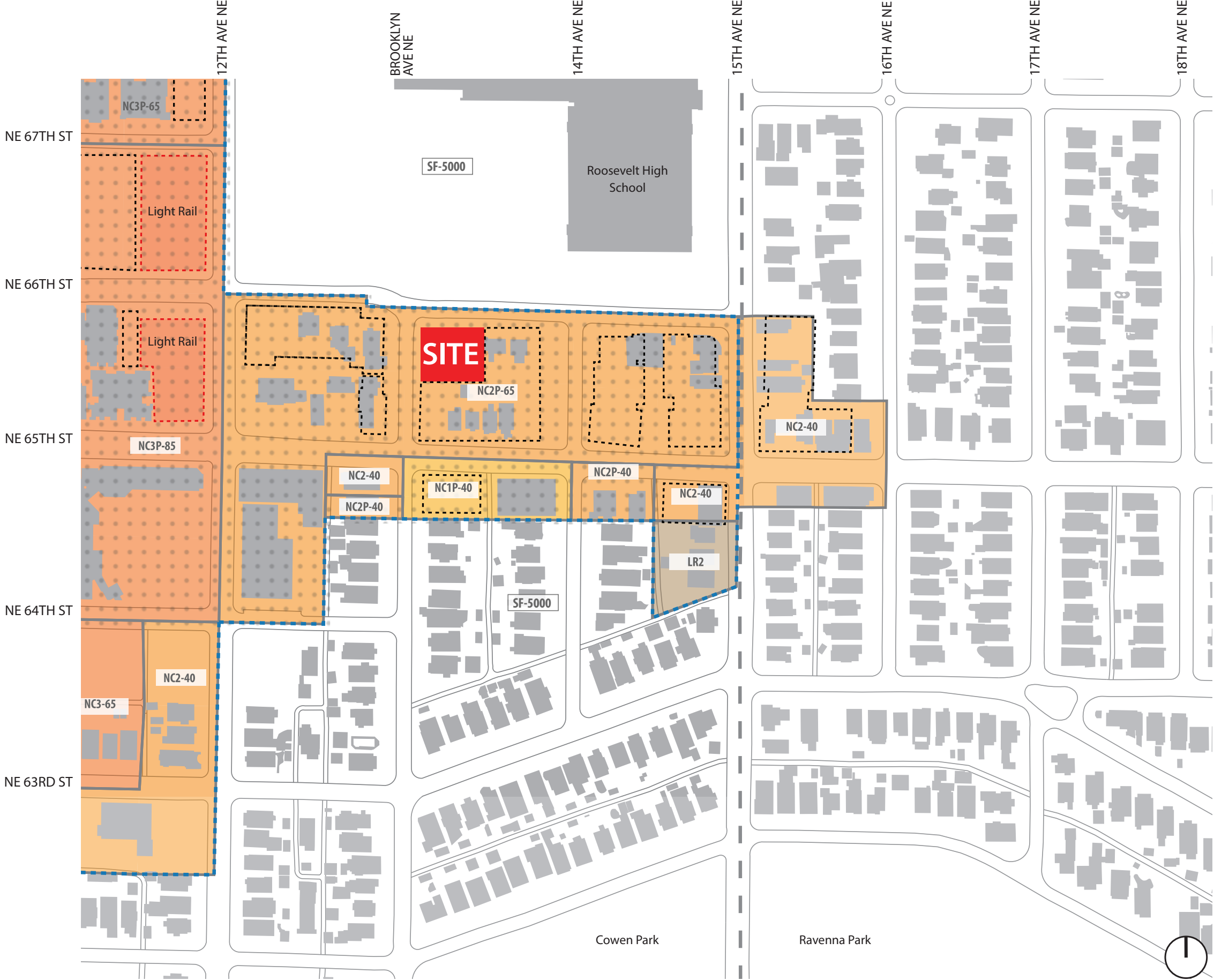
4 | 1309 NE 66th Street: Early Design Guidance

SITE CONTEXT:
EXISTING ZONING

The site and the blocks immediately to the east and west are zoned NC2P-65. The property to the north beyond the rights-of-way is SF 5000, though it is occupied by Roosevelt High School.

Existing Zoning Legend

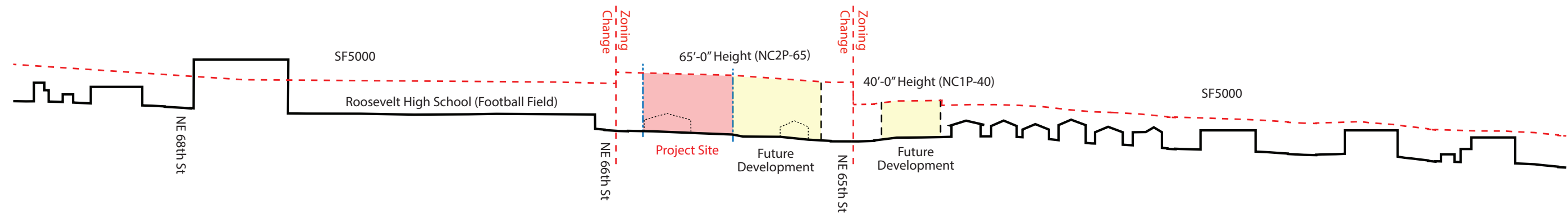
NC1	
NC2	
NC3	
LR1 & LR2	
SF	
Roosevelt Station Area Overlay	
Roosevelt Residential Urban	
Zone Boundaries	
Pedestrian Overlay	



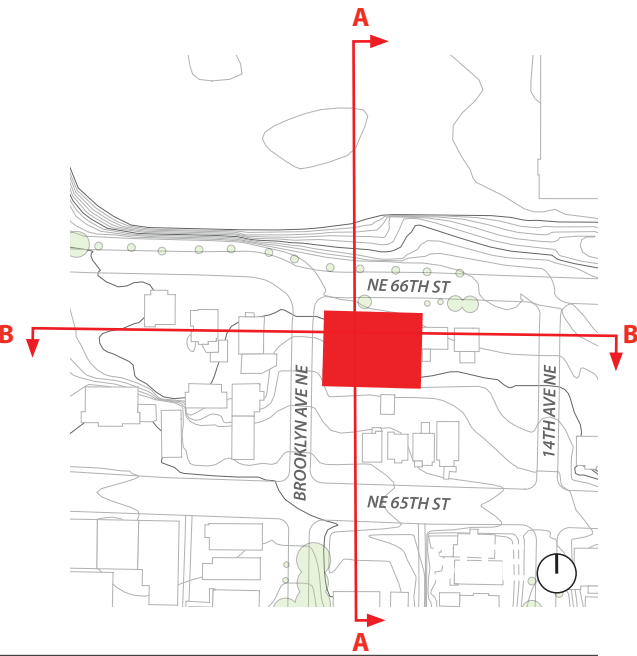
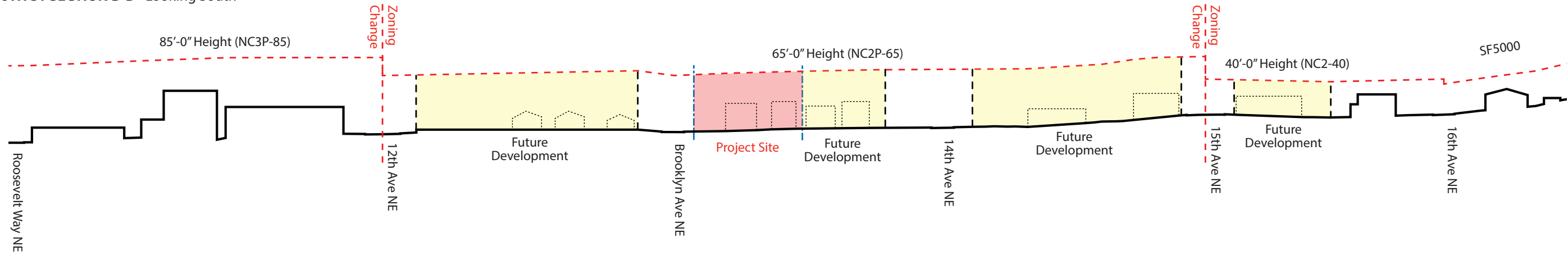
CONTEXT: FUTURE DEVELOPMENT ZONING ENVELOPES



BROOKLN AVE NE SECTION A-A - Looking East



NE 66TH ST SECTION B-B - Looking South



CONTEXT: SURROUNDING NEIGHBORHOOD

- The neighborhood surrounding the site is in transition, due in large part to the Link light rail station that will soon open at the corner of NE 65th and 12th Ave NE.
- Multiple structures are boarded up or empty, awaiting redevelopment, and a MUP has already been approved for a new mixed-use project across the street at Brooklyn and NE 66th.
- The eastern edge of the Roosevelt Neighborhood Commercial Core is to the southwest of the site.
- The Roosevelt Design Guidelines identify two neighborhood “gateway” intersections on 65th: one at the 12th Ave NE intersection and one at 15th Ave. Development should enhance the pedestrian connection between these two gateways.
- Beyond the Roosevelt Station Area Overlay, the residential areas are well-maintained, and the building style is cohesive with pitched roofs, eaves, and raised porches.
- Roosevelt High School, the largest high school in the city, is located a block north of the site. Two parks, Cowen and Ravenna, are located a few blocks south of the project.





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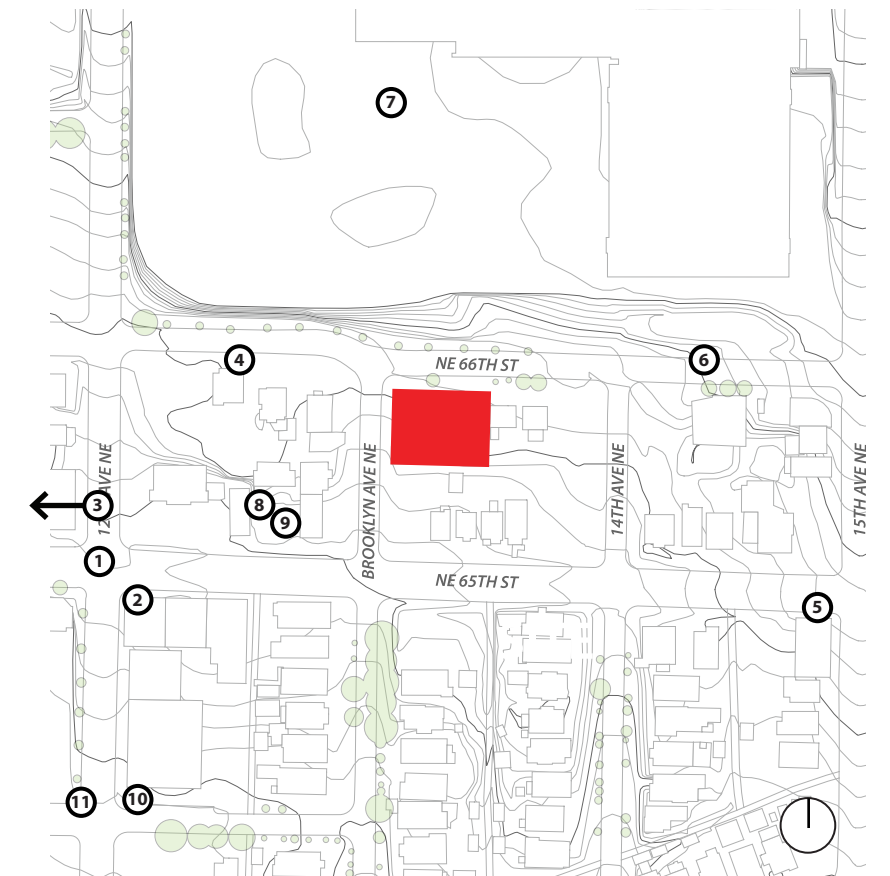
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11



1 Link Light Rail Roosevelt Station. Expected opening in 2021, located 1-1/2 blocks from the site. // **2** Retail at eastern edge of Roosevelt Commercial Core. // **3** Retail at eastern edge of Roosevelt Commercial Core. // **4** Properties on NE 66th St Awaiting Redevelopment. // **5** Rising Sun Produce at SW corner of 65th and 15th. // **6** Roosevelt High School. The school is located a block north of the site // **7** Roosevelt High School Playfield. Located directly north of the site. // **8** Commercial context along NE 65th St. // **9** Commercial context along NE 65th St. // **10** Masonry Centurylink data center. // **11** Typical Retail development along NE 65th St.

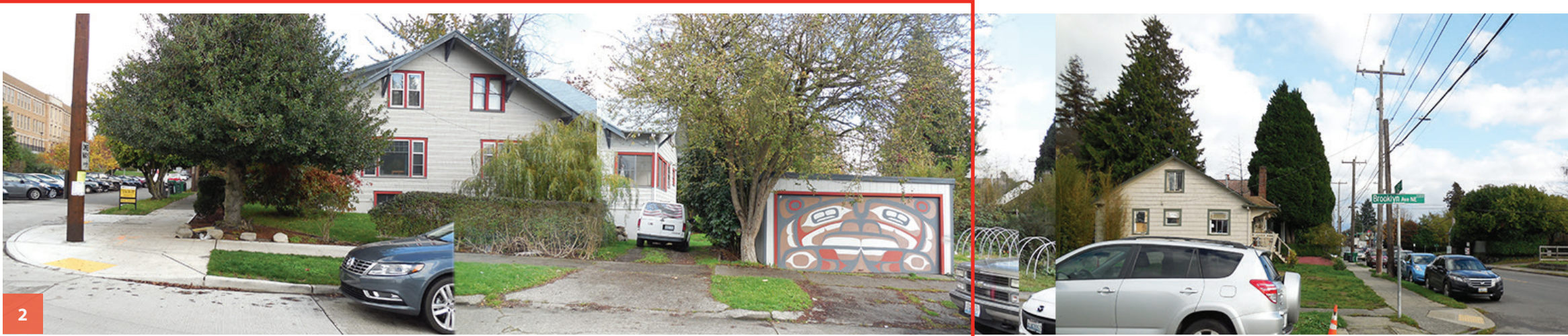
CONTEXT: BROOKLYN AVE NE

The western edge of the property is bounded by Brooklyn Ave NE. The topography along the property line has a moderate 5.5% slope, ascending to the north. The street sees less traffic than neighboring 65th and 66th and is used for street parking and a pedestrian shortcut between 65th and Roosevelt High School.



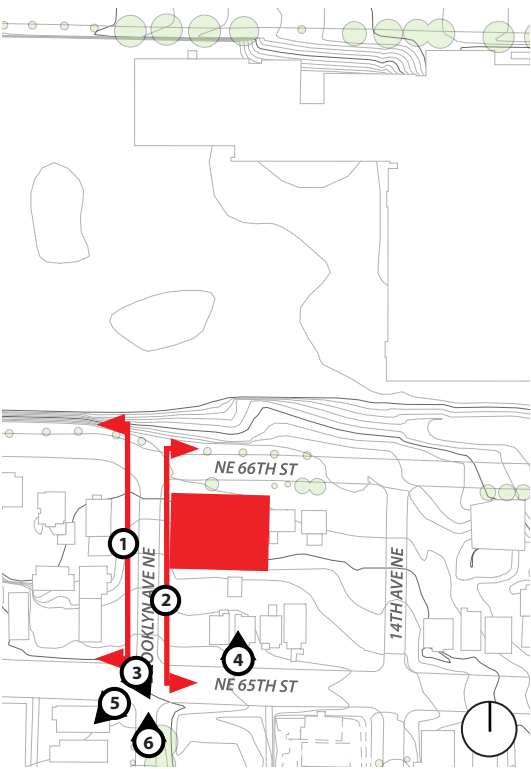
BROOKLYN AVE NE - LOOKING WEST

OPPOSITE OF PROJECT SITE



BROOKLYN AVE NE - LOOKING EAST

PROJECT SITE



CONTEXT: NE 66TH ST



NE 66TH ST - LOOKING NORTH

OPPOSITE OF PROJECT SITE

The northern edge of the property is bounded by NE 66th Street. The topography along the property line has a minimal slope (2.2%) ascending to the east.

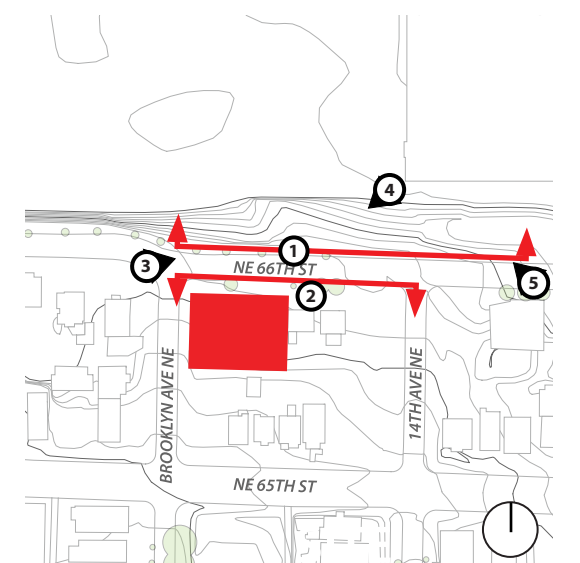
Both sides of 66th are lined with street trees, though the trees on the north side are consistent in type and spacing, framing the edge of the Roosevelt High School campus.

The south side of 66th is zoned for Neighborhood Commercial (NC) with a 65-ft height limit, while the north has the potential for Single Family residential (SF), though current property uses do not reflect this potential. Currently, the south side of the block is occupied by single-family houses and an undeveloped lot at 14th Ave E (image 4).



NE 66TH ST- LOOKING SOUTH

PROJECT SITE



EXISTING SITE PLAN & SITE ANALYSIS

Setback Requirements

- A 10-ft average ground level setback along 66th Ave NE, a minimum upper level setback of 4-ft, and 14-ft setback above 45-ft height.
- A 5-ft average ground level setback along Brooklyn Ave NE, a minimum upper level setback of 4-ft, and 9-ft setback above 45-ft height.
- Overhead power lines that run along NE 66th St require a 10-ft clearance from structures.

Traffic and Circulation

- NE 66th St is a one-way residential access street with parking on the north side across from the project site
- Brooklyn Ave NE is a residential access street with parking on both sides

Topography

- Slope along Brooklyn property line: moderate 5.5% slope, ascending to the north.
- Gentle slope along NE 66th St property line: minimal 2.2% slope ascending to the east.

Solar Access

- Limited potential solar access to the south and east anticipated after development of adjacent 7-story mixed-used projects.
- 30' to centerline of right-of-way on the north and west property lines provides good access to light and air.

Exceptional Trees

- Trees 6" or greater in diameter as measured 4.5' above the ground are identified on the site plan by diameter and species.

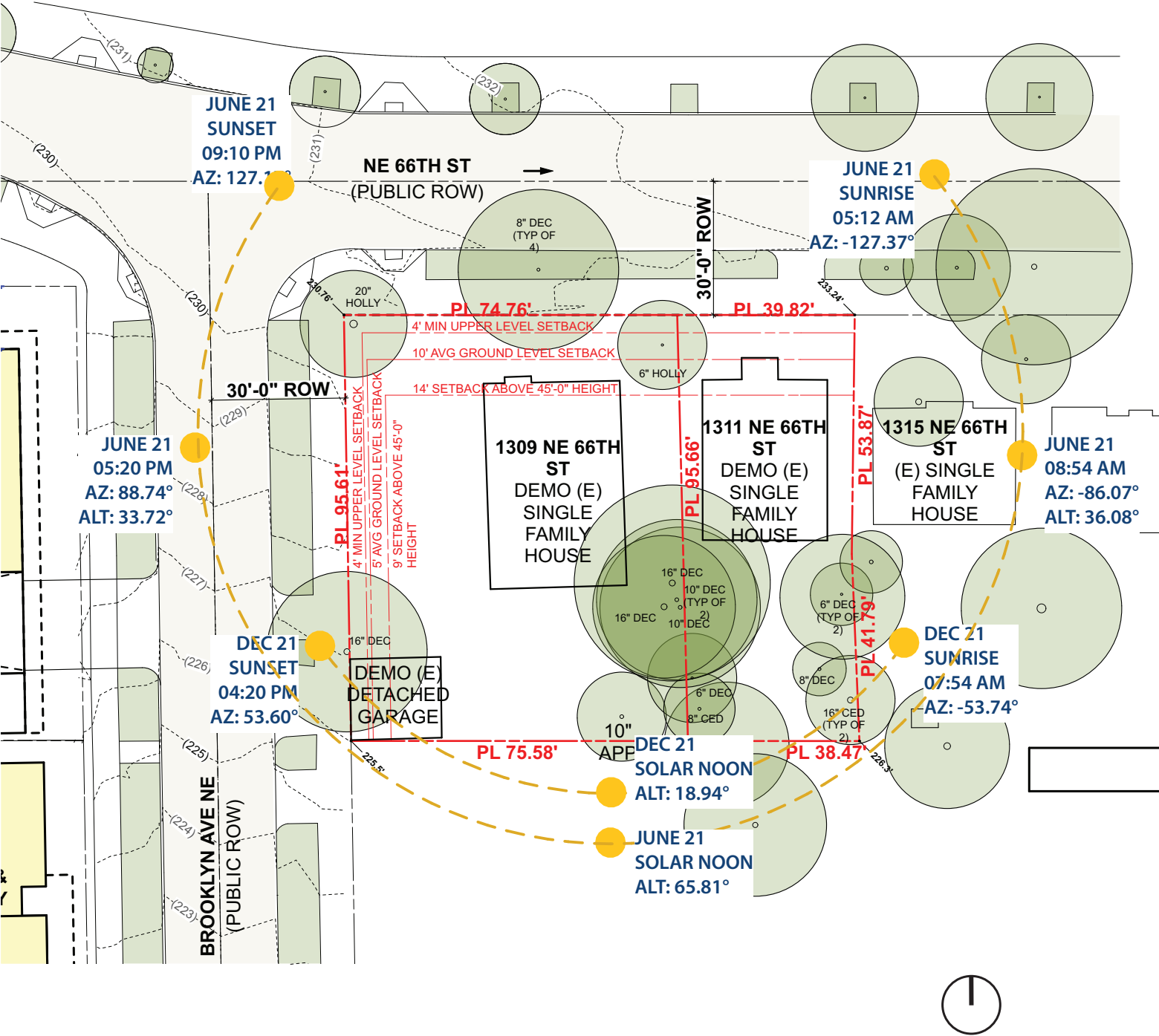
Tax ID Number

Parcel A: 052504-9032
Parcel B: 052504-9134

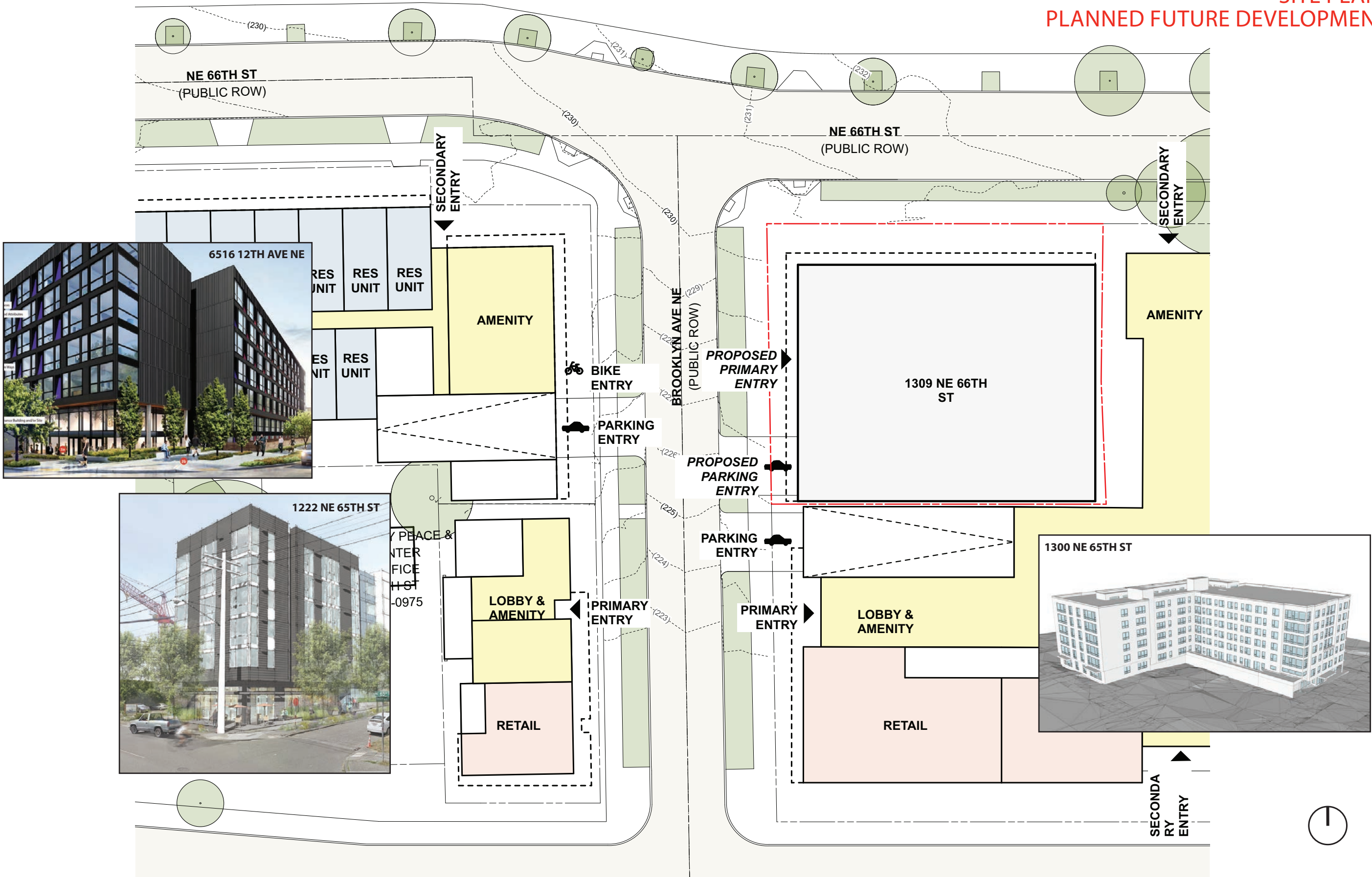
Legal Description

Parcel A: BEG 449.32 FT W & 138.25 FT N OF SE SEC COR TH N 95.65 FT TH W 40 FT TH S 95.65 FT TH E 40 FT TO BEG

Parcel A: BEG INTS S LN E 66TH ST WITH E LN BROOKLYN AVE TH E 74.70 FT TH 95.6 FT TH W 74.53 FT TH N TO BEG



SITE PLAN &
PLANNED FUTURE DEVELOPMENTS



ZONING DATA

PARCEL NO	Parcel A: 152504-9032 (1309 NE 66th St); Parcel B: 152504-9134 (1311 NE 66th St)	
LOT AREA	10,959-sf (0.25 acres)	
ZONING	NC2P-65 (4.0) Roosevelt Urban Village, Pedestrian Overlay, Roosevelt Station Overlay Distrcit (SAOD)	
PERMITTED USES	<ul style="list-style-type: none">Residential uses: permitted outright.	23.47A.004, Table A
STREET LEVEL STANDARDS	<ul style="list-style-type: none">Blank segments of street-facing façade between 2-ft to 8-ft above grade may not exceed 20-ft in width.Blank façade segments not to exceed 40% of façade width.At least one street-level street-facing façade with residential use shall have a visually prominent pedestrian entry.The floor of a dwelling unit located along the street-level street-facing facade shall be at least 4-ft above or below sidewalk grade or be set back at least 10-ft from the sidewalk.	23.47A.008
STRUCTURE HEIGHT	<ul style="list-style-type: none">Maximum structure height as zoned: 65-ft.Rooftop features: open railings, planters, clerestories, parapets, firewalls permitted to extend 4-ft above applicable height limit.Insulation material, rooftop decks and other similar features, or soil for landscaping located above the structural roof surface may exceed the maximum height limit by up to 2-ft if enclosed by compliant parapets or walls.Mechanical equipment permitted to extend 15-ft above applicable height limit, while stair and elevator penthouses are permitted to extend 16-ft, —as long as total does not exceed 20% of the roof area—or 25% if the total includes stair or elevator penthouses or screened mechanical equipment.	23.47A.012
FAR	<ul style="list-style-type: none">Applies to all structures in NC zones.Allowable FAR for in sites zoned with a 65' height limit located within a SAOD: 5.75.FAR exceeding the incentive zoning suffix is required to comply with SMC 23.58A.Gross floor area not counted against FAR:<ul style="list-style-type: none">Areas underground;Portions of a story extending no more than 4-ft above grade (lower of existing or finished grade).Minimum required FAR for new construction: 2.0.	23.47A.013
SETBACKS	<ul style="list-style-type: none">No residential lots abutting or across an alley from the site.Structures allowed in required setbacks: decks with open railings; ramps/devices for egress; fences and freestanding walls ≤ 6-ft above lower of finished/existing grade; retaining walls ≤ 6-ft above existing grade; and, with restrictions, green stormwater infrastructure (GSI). (See Page 12, SMC 2347A.009, for site specific setback requirements.)	23.47A.014
AMENITY AREA	<ul style="list-style-type: none">Residential amenity area equal to 5% of residential gfa:<ul style="list-style-type: none">All residents have access to at least one common or private amenity area;Amenity areas to be unenclosed;Common residential amenity areas shall have a dimension of at least 10-ft and a minimum area of 250-sf;Private balconies and decks to have a dimension of at least 6-ft and an minimum area of 60-sf.	23.47A.024
PARKING ACCESS	<ul style="list-style-type: none">If access is not from alley and site abuts two or more streets access permitted to cross one of the side street lot lines per SMC 23.47A.032.C with curb cuts per SMC 23.54.030.F.2.a.1.Each permitted curb cut allows one street-facing garage door not to exceed maximum width of curb cut.Director to determine which street is the front lot line when lot fronts on two or more streets.	23.47A.032
PARKING	<ul style="list-style-type: none">Parking for Residential Uses per Table B.<ul style="list-style-type: none">No parking required for residential in urban centers or SAODBicycle parking required per Table D.<ul style="list-style-type: none">Multi-family structures, 1/4DUs long-term, no short-term	23.54.015
PARKING STANDARDS	<ul style="list-style-type: none">Driveway slope: No portion of a driveway... shall exceed a slope of 15%.Sight triangle required at both sides of two way driveways < 22-ft wide, clear of obstructions 10-ft from intersection of driveway with sidewalk.Sight triangle required at exit side of two way driveways ≥ 22-ft wide, clear of obstructions 10-ft from intersection of driveway with sidewalk.	23.54.030

PL1 Connectivity <i>Complement and contribute to the network of open spaces around the site and the connections among them</i>	
	B3 Walkways and Connections, Pedestrian Amenities Opportunities for creating lively, pedestrian oriented open spaces to enliven the area and attract interest and interaction with the site and building should be considered. Visible access to the building’s entry should be provided. Examples of pedestrian amenities include seating, other street furniture, lighting, year-round landscaping, seasonal plantings, pedestrian scale signage, site furniture, art work, awnings, large storefront windows, and engaging retail displays and/ or kiosks
Roosevelt I, Supplemental iii Guidance	A Network of Public Spaces On the blocks adjacent to the high school, anticipate the movement of large groups between the school grounds and commercial areas in order to design for pedestrian safety along 12th Avenue NE and NE 65th St.; the key arterials traversed by sometimes distracted students. Anticipate use of gathering spaces by groups of students. Incorporate trash collection and recycling accommodations as appropriate
PL2 Walkability <i>Create a safe and comfortable walking environment that is easy to navigate and well-connected to existing pedestrian walkways and features</i>	
	B1 Safety and Security, Eyes on the Street Create a safe environment by providing lines of sight and encouraging natural surveillance through strategic placement of doors, windows, balconies and street-level uses
	B3 Safety and Security, Street-level Transparency Ensure transparency of street-level uses (for uses such as nonresidential uses or residential lobbies), where appropriate, by keeping views open into spaces behind walls or plantings, at corners, or along narrow passageways. Choose semi-transparent rather than opaque screening
Roosevelt I, Supplemental ii Guidance	Pedestrian Experience Provide pedestrian scaled lighting on streets with direct access to the light rail station, near the High School, and on neighborhood green streets and/or greenways. These streets include 12th Ave NE, NE 66th, NE 67th, and NE 68th Streets.
Roosevelt I, Supplemental iii Guidance	Pedestrian Experience Pedestrian amenities are encouraged where appropriate along sidewalks within the commercial core. Amenities should be placed within setbacks. Examples of amenities include: <ul style="list-style-type: none">■ Trash & recycling■ Canopies■ Seating■ Drinking water fountains■ Artwork■ Special surface treatments■ Plantings■ Pedestrian scaled lighting■ Courtyards
PL3 Street-Level Interaction <i>Encourage human interaction and activity at the street-level with clear connections to building entries and edges</i>	
	A1 Entries, Design Objectives Design primary entries to be obvious, identifiable, and distinctive with clear lines of sight and lobbies visually connected to the street. Scale and detail them to function well for their anticipated use and also to fit with the building of which they are a part, differentiating residential and commercial entries with design features and amenities specific to each.

POTENTIAL DESIGN GUIDELINES

	<p>B2 Residential Edges, Ground Level Residential</p> <p>Privacy and security issues are particularly important in buildings with ground-level housing, both at entries and where windows are located overlooking the street and sidewalk. Consider providing a greater number of transition elements and spaces, and choose materials carefully to clearly identify the transition from public sidewalk to private residence. In addition to the ideas in PL3.B1, design strategies include:</p> <ul style="list-style-type: none">a. vertical modulation and a range of exterior finishes on the facade to articulate the location of residential entries;b. pedestrian-scaled building addressing and signage, and entry elements such as mail slots/boxes, doorbells, entry lights, planter boxes or pots; andc. a combination of window treatments at street level, to provide solutions to varying needs for light, ventilation, noise control, and privacy.
	<p>C2 Retail Edges, Visibility</p> <p>Maximize visibility into the building interior and merchandise displays. Consider fully operational glazed wall-sized doors that can be completely opened to the street, increased height in lobbies, and/ or special lighting for displays</p>
<p>Roosevelt I, Supplemental i Guidance</p>	<p>High School, Green Streets, and Green Ways</p> <p>Provide a more intimate, smaller-scale residential environment on the blocks adjacent to the high school by providing landscaping, stoops, porches, etc.</p>
<p>DC2 Architectural Concept <i>Develop an architectural concept that will result in a unified & functional design that fits well on the site & within its surroundings</i></p>	
	<p>A1 Massing, Site Characteristics and Uses</p> <p>Arrange the mass of the building taking into consideration the characteristics of the site and the proposed uses of the building and its open space. In addition, special situations such as very large sites, unusually shaped sites, or sites with varied topography may require particular attention to where and how building massing is arranged as they can accentuate mass and height</p>
	<p>A2 Massing, Reducing Perceived Mass</p> <p>Use secondary architectural elements to reduce the perceived mass of larger projects. Consider creating recesses or indentations in the building envelope; adding balconies, bay windows, porches, canopies or other elements; and/or highlighting building entries.</p>
	<p>B1 Architectural and Facade Composition, Facade Composition</p> <p>Design all building facades—including alleys and visible roofs—considering the composition and architectural expression of the building as a whole. Ensure that all facades are attractive and wellproportioned through the placement and detailing of all elements, including bays, fenestration, and materials, and any patterns created by their arrangement. On sites that abut an alley, design the alley façade and its connection to the street carefully. At a minimum, consider wrapping the treatment of the street-facing façade around the alley corner of the building.</p>
	<p>C3 Secondary Architectural Features, Fit with Neighboring Buildings</p> <p>Use design elements to achieve a successful fit between a building and its neighbors, such as:</p> <ul style="list-style-type: none">a. consider aspects of neighboring buildings through architectural style, roof line, datum line detailing, fenestration, color or materials,b. using trees and landscaping to enhance the building design and fit with the surrounding context, and/orc. creating a well-proportioned base, middle and top to the building in locations where this might be appropriate. Consider how surrounding buildings have addressed base, middle, and top, and whether those solutions-- or similar ones-- might be a good fit for the project and its context.
	<p>D1 Scale and Texture, Human Scale</p> <p>Incorporate architectural features, elements, and details that are of human scale into the building facades, entries, retaining walls, courtyards, and exterior spaces in a manner that is consistent with the overall architectural concept. Pay special attention to the first three floors of the building in order to maximize opportunities to engage the pedestrian and enable an active and vibrant street front.</p>

In all designs landscaping, site walls and/or fencing, and steps will be used as transitional elements between the ground level residential and public sidewalk along NE 66th St. Additional elements of window treatments and landscape details will be further refined as the design progresses.

The combination of public campus-like landscaping, tree-lined street, and pedestrian amenities with the private, individual residential gated yards with transitional landscaping will provide a small-scale residential environment along NE 66th St.

Roosevelt II, Supplemental ii Guidance	Architectural and Façade Composition Along Green streets, Greenways, and Non-Arterial streets: a. Maximize modulation, courtyards, human interaction; b. Incorporate high quality materials, a mix of informal planting, and integration of natural materials, especially at the entries.
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DC3 Open Space Concept

Integrate open space design with the design of the building so that each complements the other

- B4 Open Space Uses and Activities, Multifamily Open Space**
Design common and private open spaces in multi-family projects for use by all residents to encourage physical activity and social interaction. Some examples include areas for gardening, children’s play (covered and uncovered), barbeques, resident meetings, and crafts or hobbies
- C2 Design, Amenities and Features**
Create attractive outdoor spaces well-suited to the uses envisioned for the project. Use a combination of hardscape and plantings to shape these spaces and to screen less attractive areas as needed. Use a variety of features, such as planters, green roofs and decks, groves of trees, and vertical green trellises along with more traditional foundation plantings, street trees, and seasonal displays

Roosevelt II, Supplemental ii Guidance	Street Planting & Landscape to Enhance the Building and/or Site Landscaping should be employed as both a design feature and an environmental enhancement. Dominant street tree varieties from the neighborhood should be incorporated into the plan.
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As the project is developed further these guidelines will influence landscape choices.

DC4 Exterior Elements and Finishes

Use appropriate and high-quality elements and finishes for the building and its open spaces

- A1 Building Materials, Exterior Finish Materials**
Building exteriors should be constructed of durable and maintainable materials that are attractive even when viewed up close. Materials that have texture, pattern, or lend themselves to a high quality of detailing are encouraged

As the project is developed further these guidelines will influence façade finishes and landscape choices.

D4 Trees Landscape and Hardscape Materials, Place Making	Create a landscape design that helps define spaces with significant elements such as trees
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Roosevelt I Supplemental Guidance	Exterior Finish Materials i. In the commercial core consider including masonry materials befitting the heritage of early 20th century commercial structures in the neighborhood (e.g. Roosevelt High School’s masonry façade). ii. The use of high-quality cladding materials, such as brick and terra cotta masonry; tile; natural and cast stone is strongly encouraged along commercial frontages, and scaled to pedestrian activity and scale, especially at the base and ground-levels. Concrete Masonry Units and high-quality concrete are also preferred over wood, metal, or cement-board claddings. iii. Colors should be consistent with and chosen based on existing architectural cues and should be considered in terms of their relationship to neighboring structures. iv. The use of more natural elements, such a brick, wood, etc. that feels welcoming to pedestrians (see Ballard Ave. as example) or high quality, durable modern elements is encouraged. v. Transparent, rather than reflective, windows facing the street are preferred. vi. Use of transparent awnings is preferred in the commercial core.
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Roosevelt IV Supplemental Guidance	Landscape Materials i. Neighborhood plant choices should consider historical landacape elements. ii. Preferred species for street trees are Tupelo ‘Afterburner’ or, in powerline locations, Dogwood ‘White Wonder’ or Katsura. iii. Indigenous trees should be planted to maintain and reinvigorate a verdant tree canopy within the neighborhood.
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DESIGN PROPOSAL: ALTERNATE 1 (NO DEPARTURES)

The proposed code-compliant alternative takes a prescriptive approach in applying the required setbacks on NE 66th St and Brooklyn Ave NE. The sloping site demands additional massing moves, which introduce variation in the setbacks above the street level.

Primary Disadvantages/Concerns

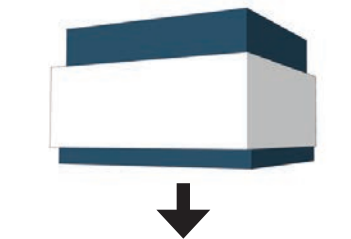
- Though setbacks meet the letter of the code, the resulting massing is out of character with the adjacent planned developments, which propose greater setbacks at street level and uniform setbacks for all upper floors (see PLANNED FUTURE DEVELOPMENTS, page 13).
- The depth of the code-compliant floor plates creates deep units on four floors with limited daylight access.

Summary	
Stories:	7 (5-over-2 + 2 below grade)
Unit Count:	71
Floor Area:	55,750-sf Residential (FAR) 19,800-sf Parking
Parking:	38 stalls

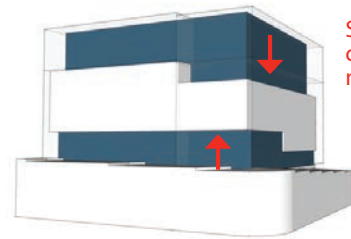
Potential Departures (Administrative)

- SMC 23.54.030.D3 Driveway slope

MASSING SETBACKS PER CODE

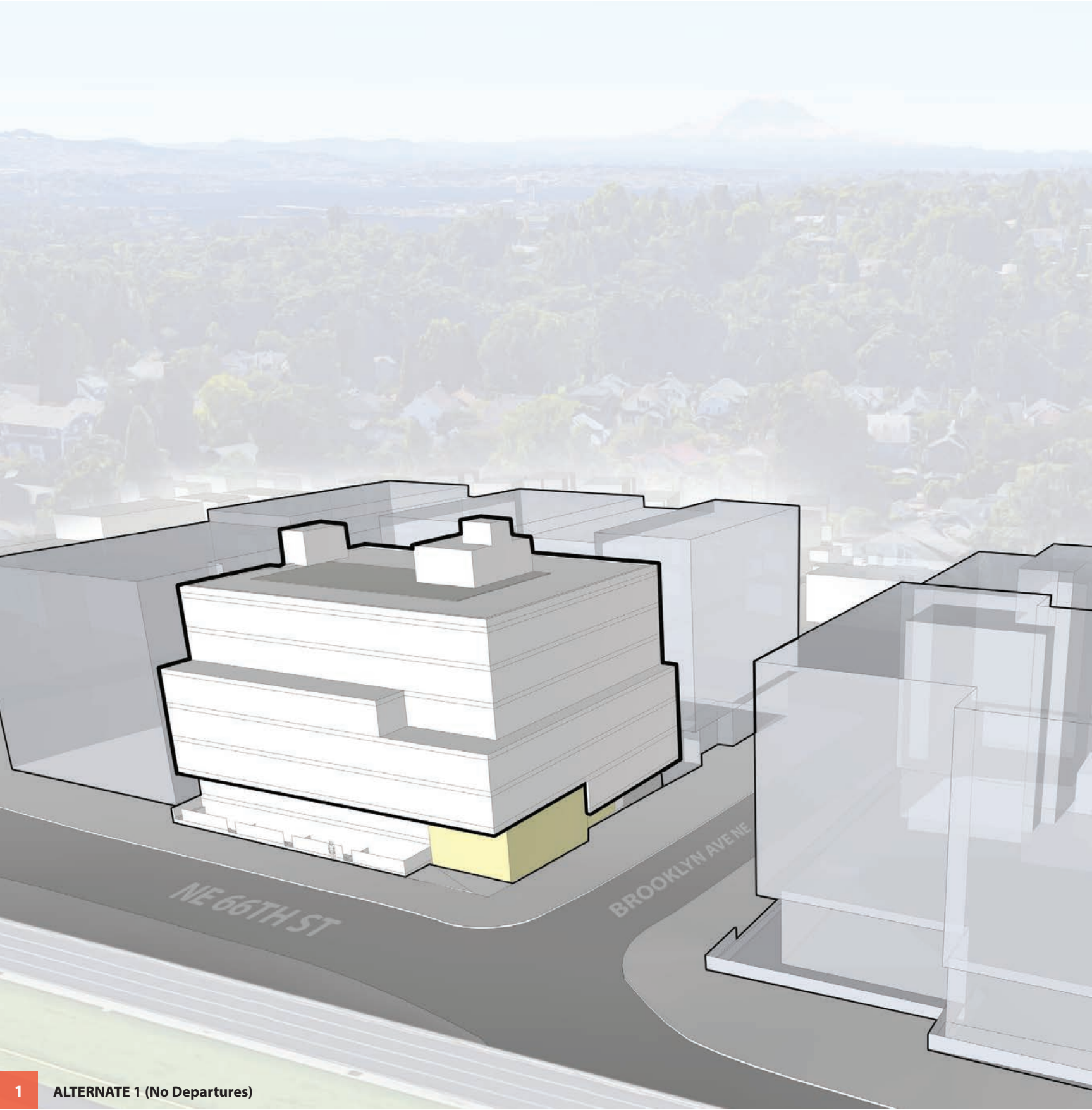
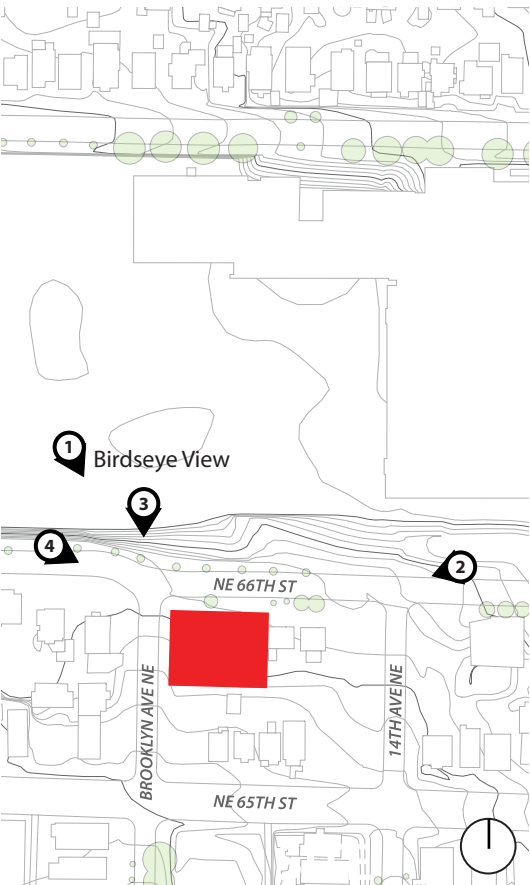


SITE-SPECIFIC ADJUSTMENTS

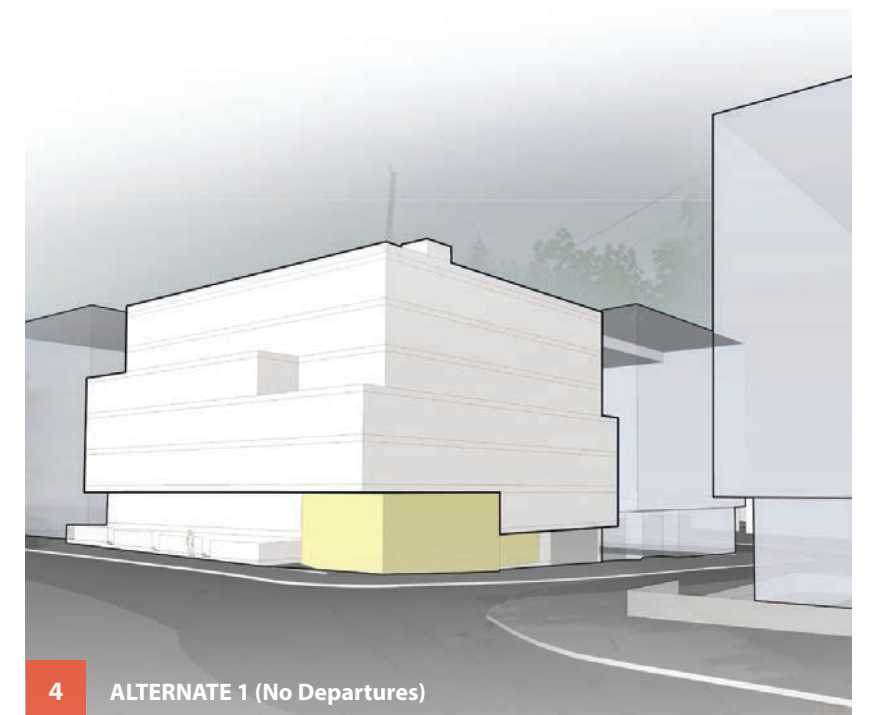
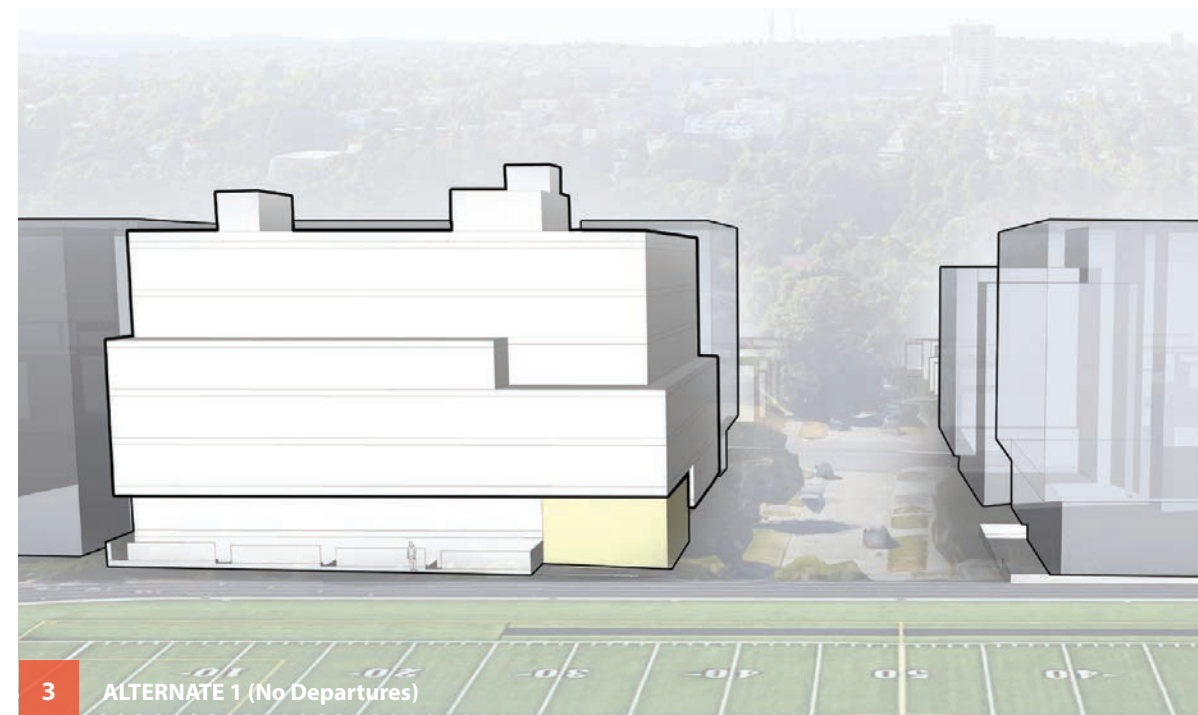
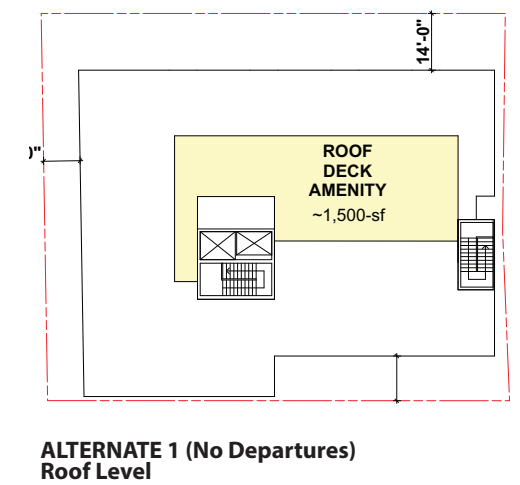
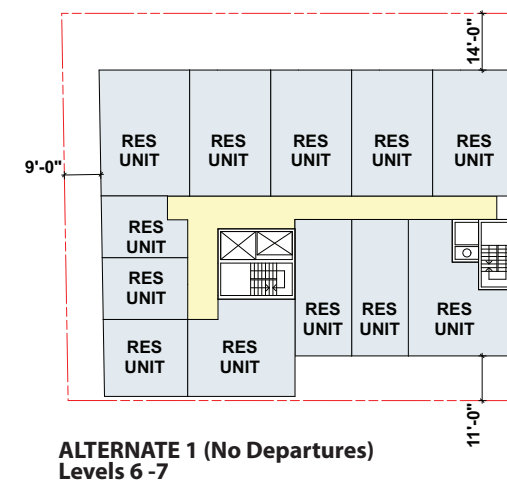
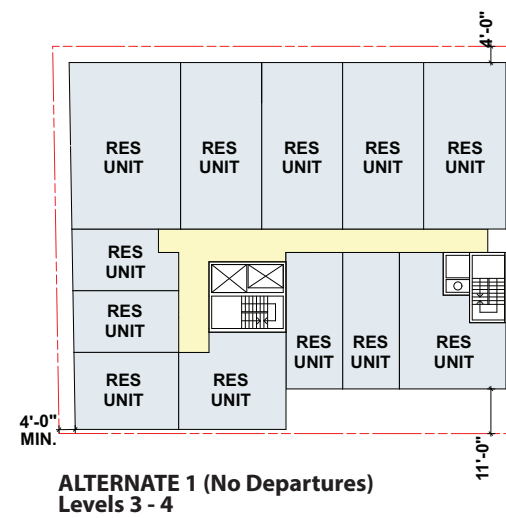
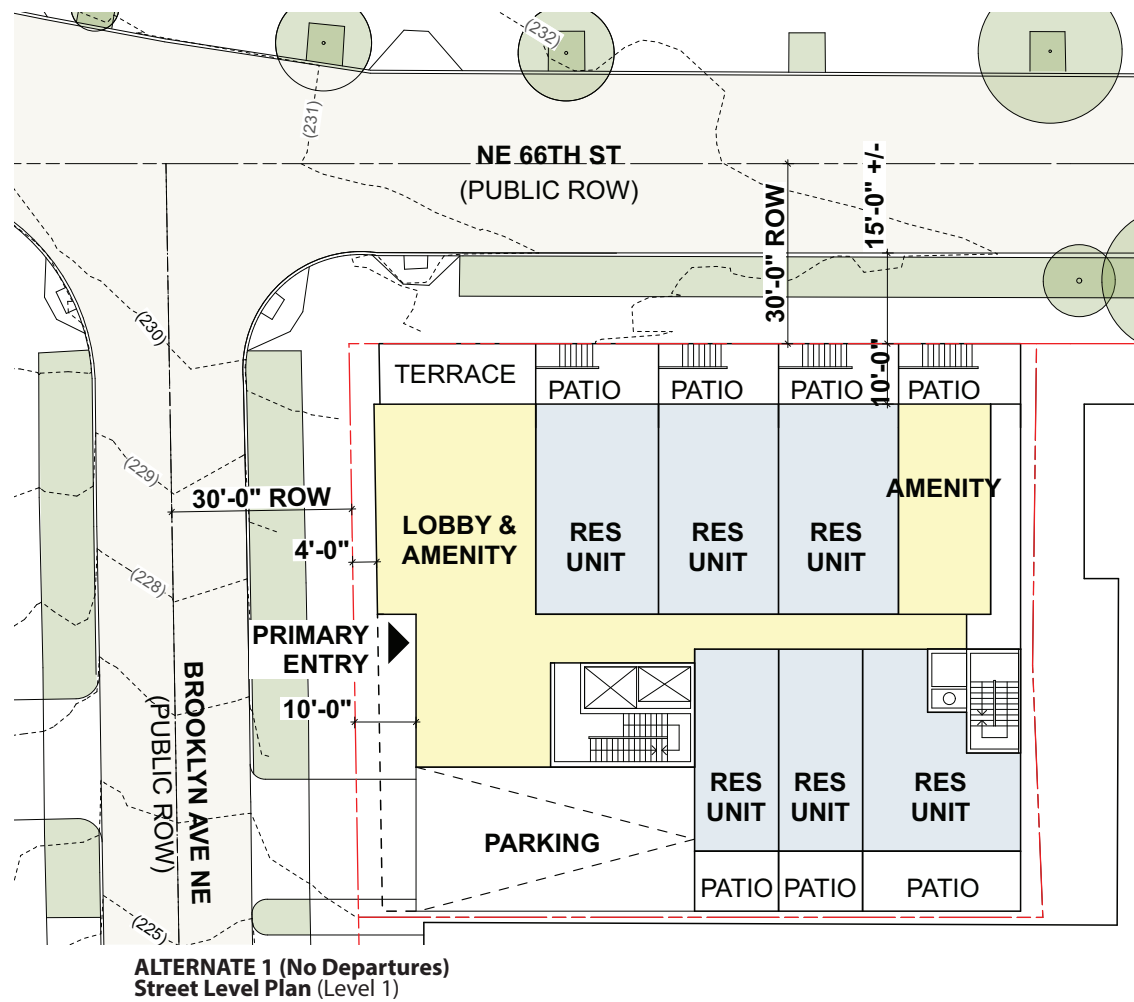


Step massing to conform to height measurement

Lift floor line to respond to sloping site



1 ALTERNATE 1 (No Departures)



DESIGN PROPOSAL: ALTERNATE 2

Setbacks consistent with adjacent planned developments are proposed for Option 2 (see PLANNED FUTURE DEVELOPMENTS, page 13). The lower two floors are setback in excess of the code requirement in order to reduce the perceived mass at the pedestrian level. Consistent floor plates at the upper level balance unit depth and modulation of the massing.

The proposed setbacks and the resulting rhythm of the massing do not increase the developable floor area beyond the code compliant alternative.

Primary Disadvantages/Concerns

- Lack of lobby and amenity spaces on Brooklyn Ave NE limits the potential to activate the street

Summary	
Stories:	7 (5-over-2 + 2 below grade)
Unit Count:	71
Floor Area:	52,800-sf Residential (FAR) 19,800-sf Parking
Parking:	38 stalls

- Potential Departures**
- SMC 23.47A.009.1a.1 Setback requirements at NE 66th St
 - SMC 23.47A.008.A3 Street-level, street-facing facades at NE 66th St
 - SMC 23.47A.009.1a.2 Setback requirements at Brooklyn Ave NE
 - SMC 23.47A.008.D2 Street-Level Street-Facing facades at Brooklyn Ave NE
 - SMC 23.54.030.G1 Sight Triangle
 - SMC 23.54.030.D3 Driveway slope

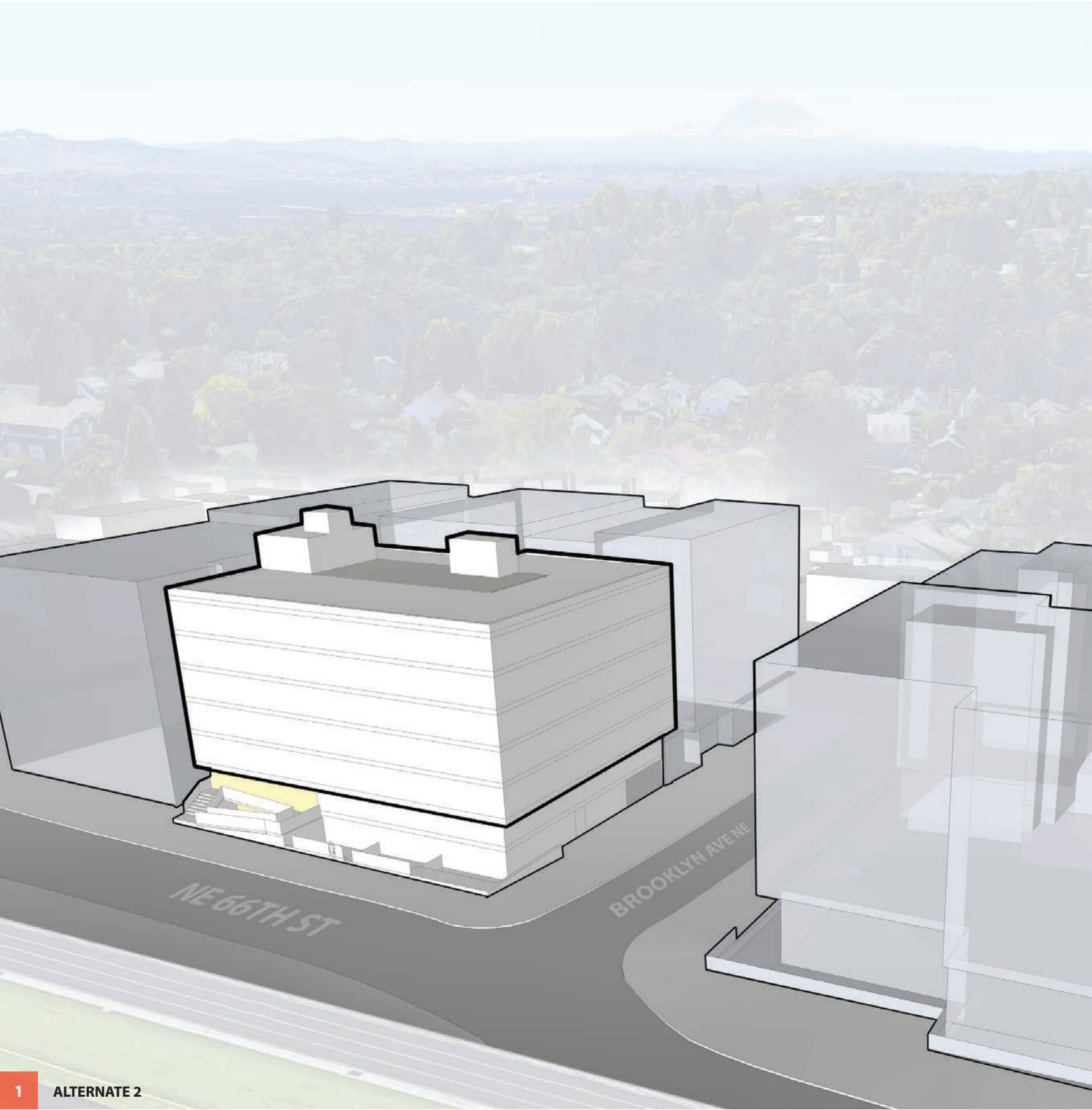
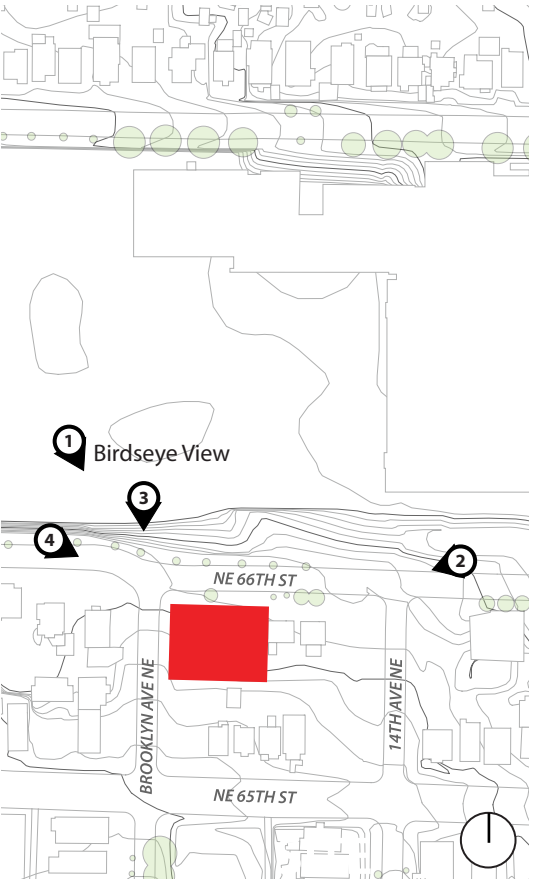
MASSING SETBACKS PER CODE



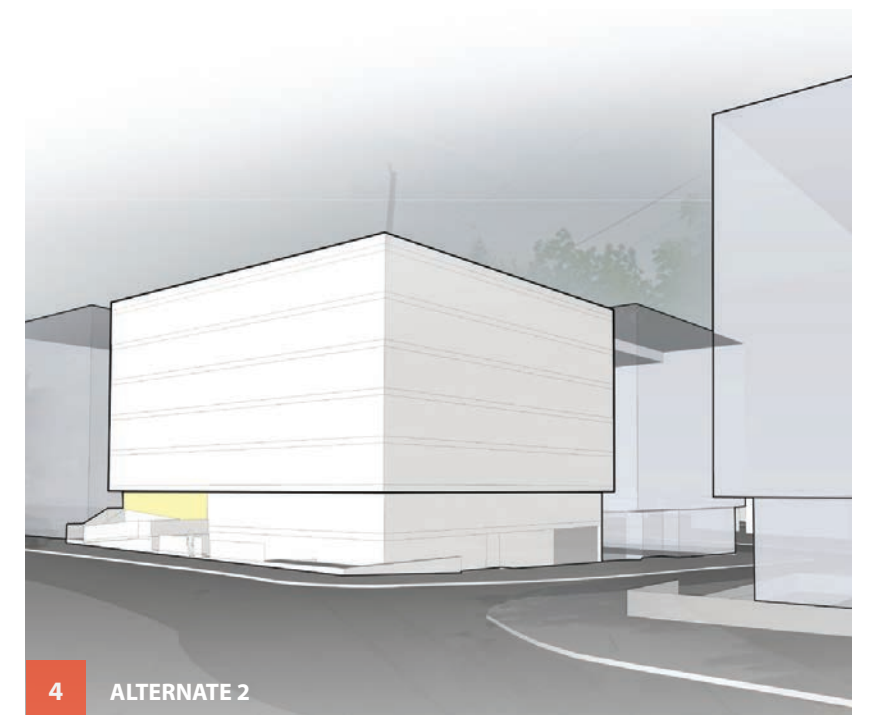
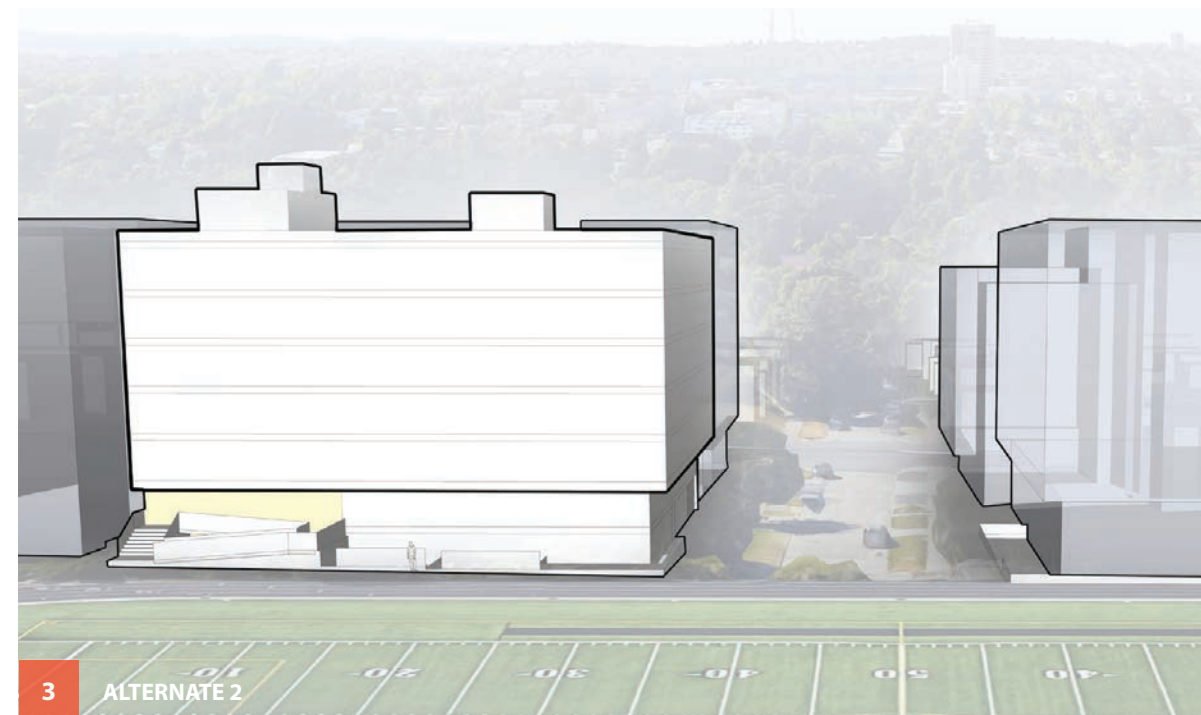
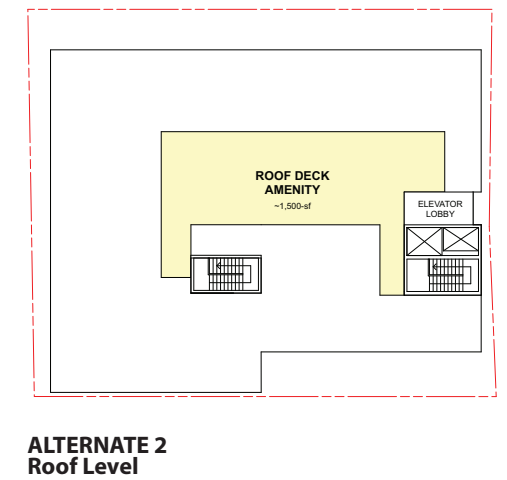
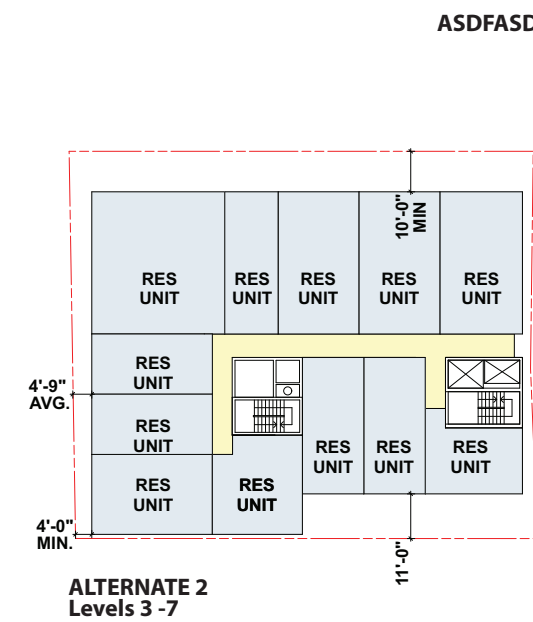
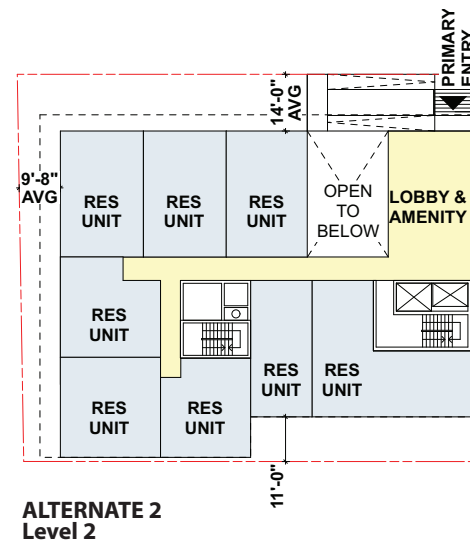
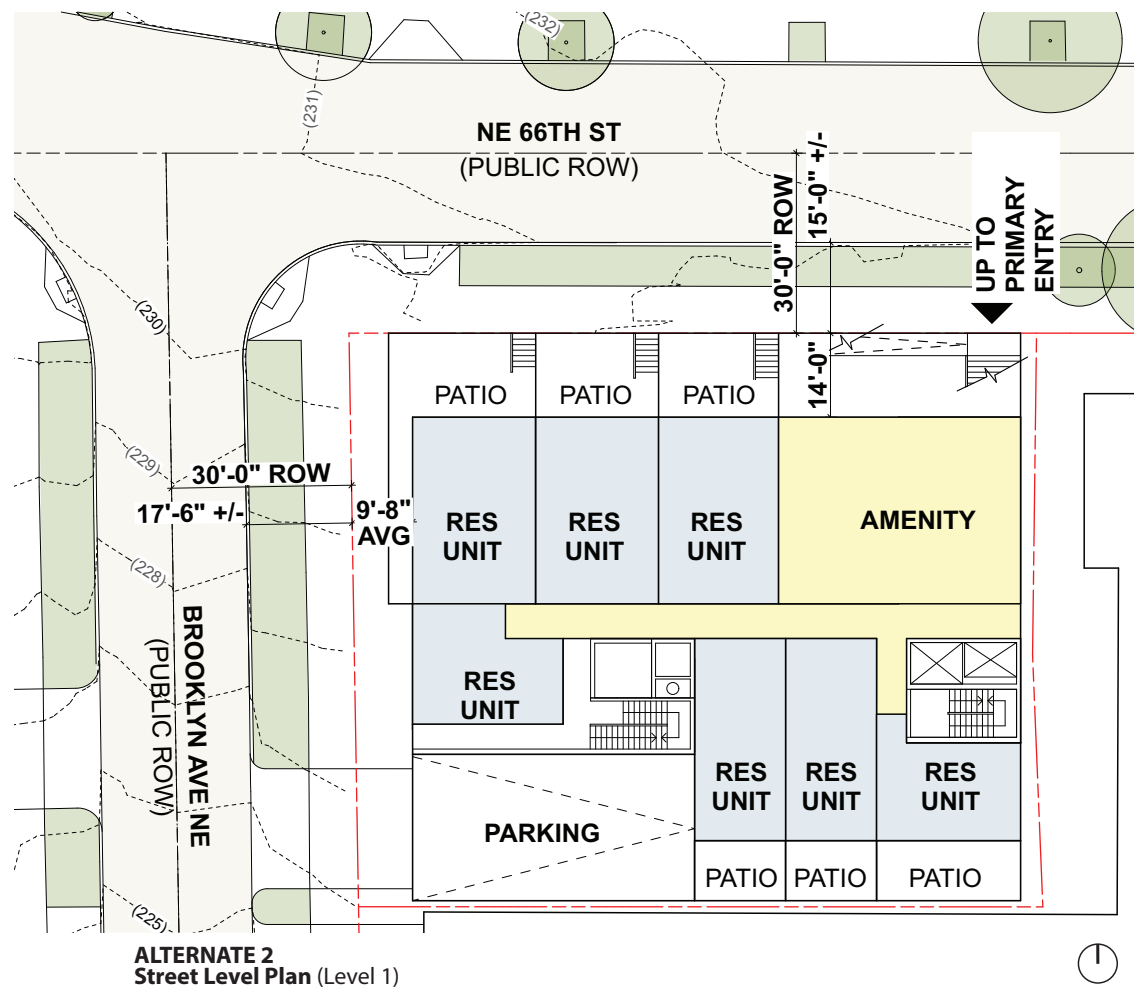
PROPOSED SETBACKS



Distinct base and top with increased lower level setbacks



1 ALTERNATE 2



DESIGN PROPOSAL: ALTERNATE 3 (PREFERRED)

Alternate 3 proposes massing similar to Alternate 2 but locates the lobby and primary amenity spaces on Brooklyn Ave NE, where they can activate the street. This configuration is consistent with the adjacent planned developments (see PLANNED FUTURE DEVELOPMENTS, page 13).

The proposed setbacks and the resulting rhythm of the massing do not increase the developable floor area beyond the code compliant alternative.

Primary Disadvantages/Concerns

- Pedestrian experience along NE 66th St

Summary

Stories:	7 (5-over-2 + 2 below grade)
Unit Count:	71
Floor Area:	52,800-sf Residential (FAR) 19,800-sf Parking
Parking:	38 stalls

Potential Departures

- SMC 23.47A.009.1a.1 Setback requirements at NE 66th St
- SMC 23.47A.008.A3 Street-level, street-facing facades at NE 66th St
- SMC 23.47A.009.1a.2 Setback requirements at Brooklyn Ave NE
- SMC 23.54.030.G1 Sight Triangle
- SMC 23.54.030.D3 Driveway slope

MASSING SETBACKS PER CODE



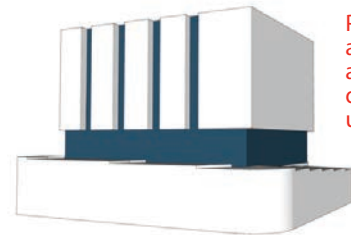
PROPOSED SETBACKS



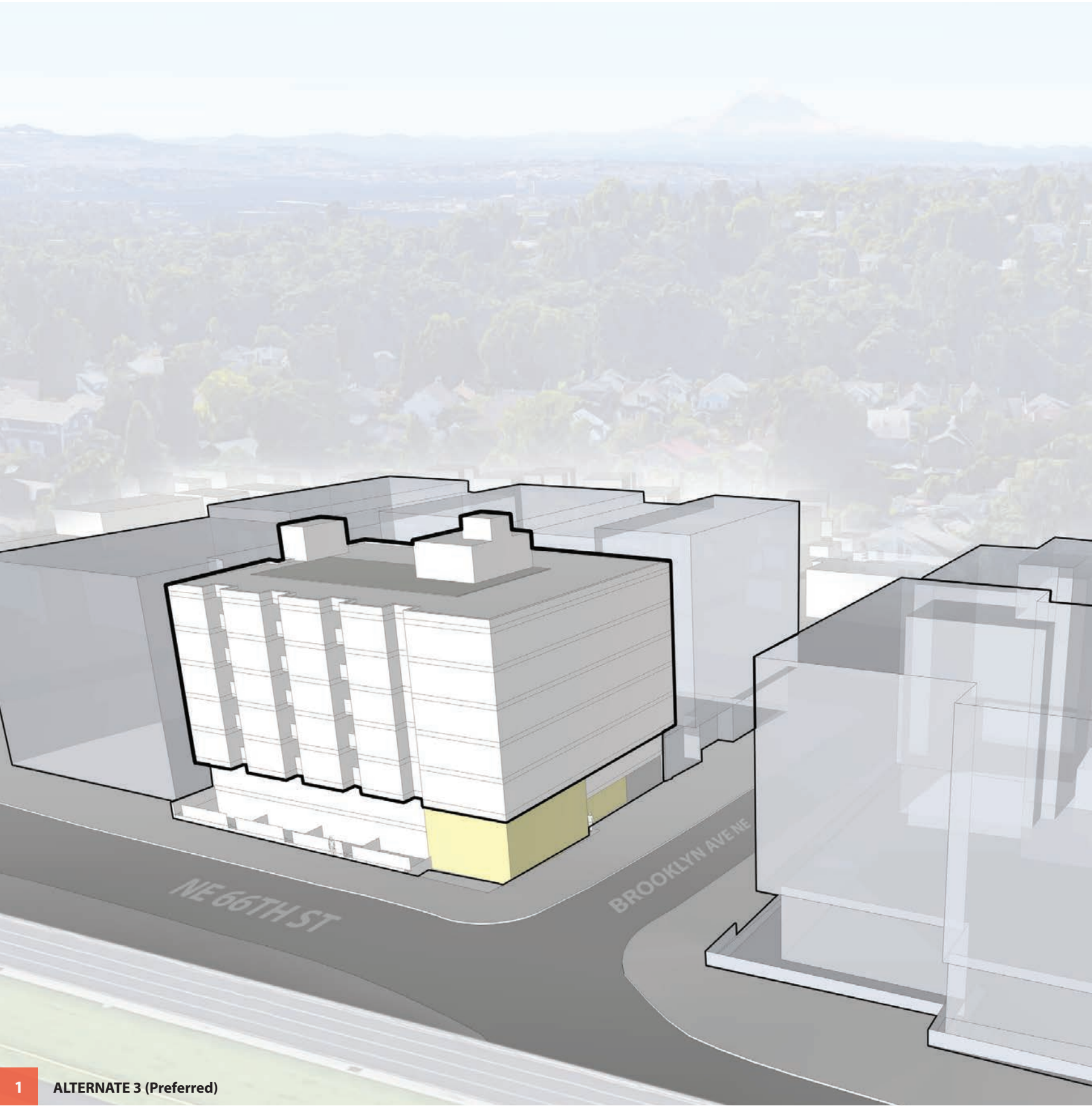
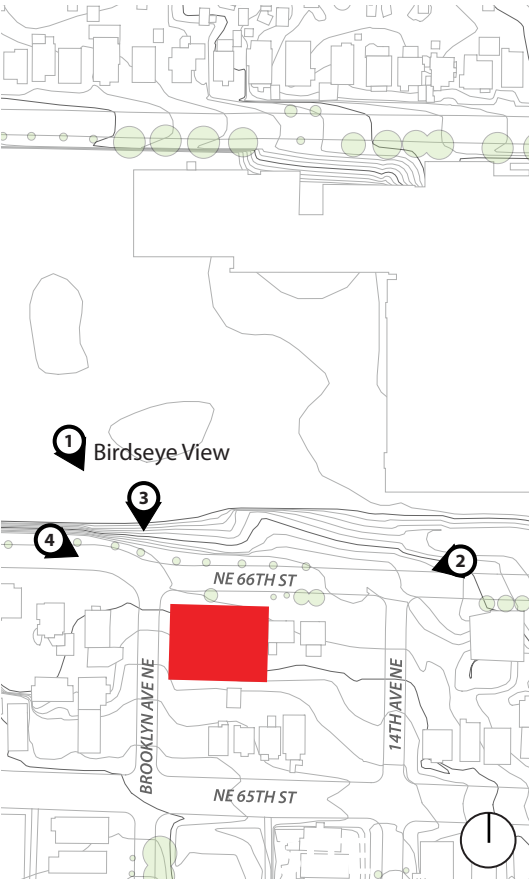
Distinct base and top with increased lower level setbacks



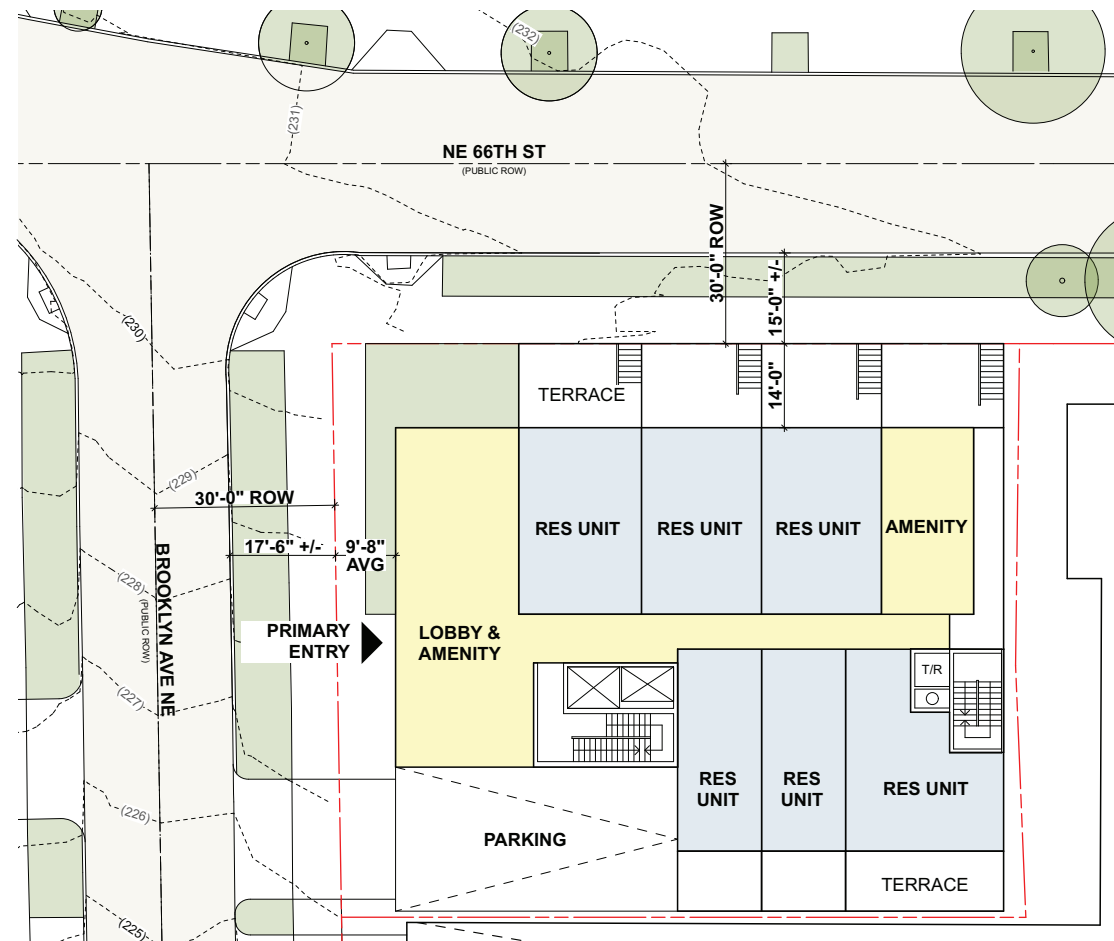
FUNCTIONAL RHYTHM



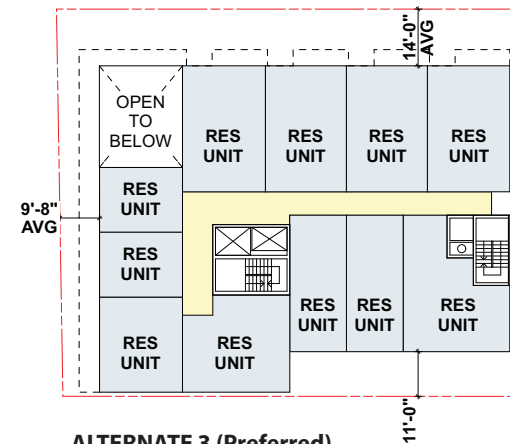
Punctuation adds interest and improves daylighting in units



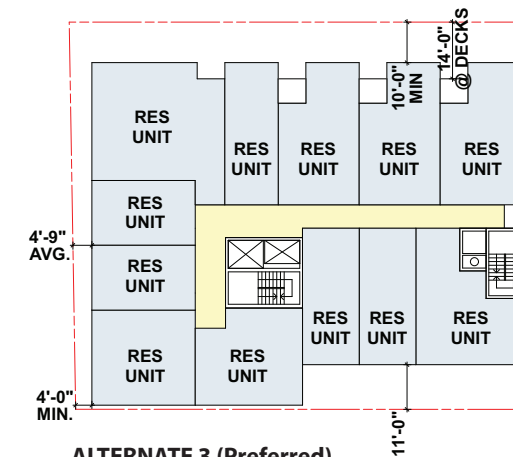
1 ALTERNATE 3 (Preferred)



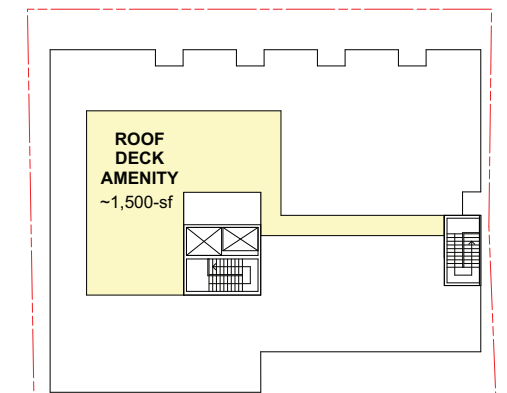
ALTERNATE 3 (Preferred)
Street Level Plan (Level 1)



ALTERNATE 3 (Preferred)
Level 2



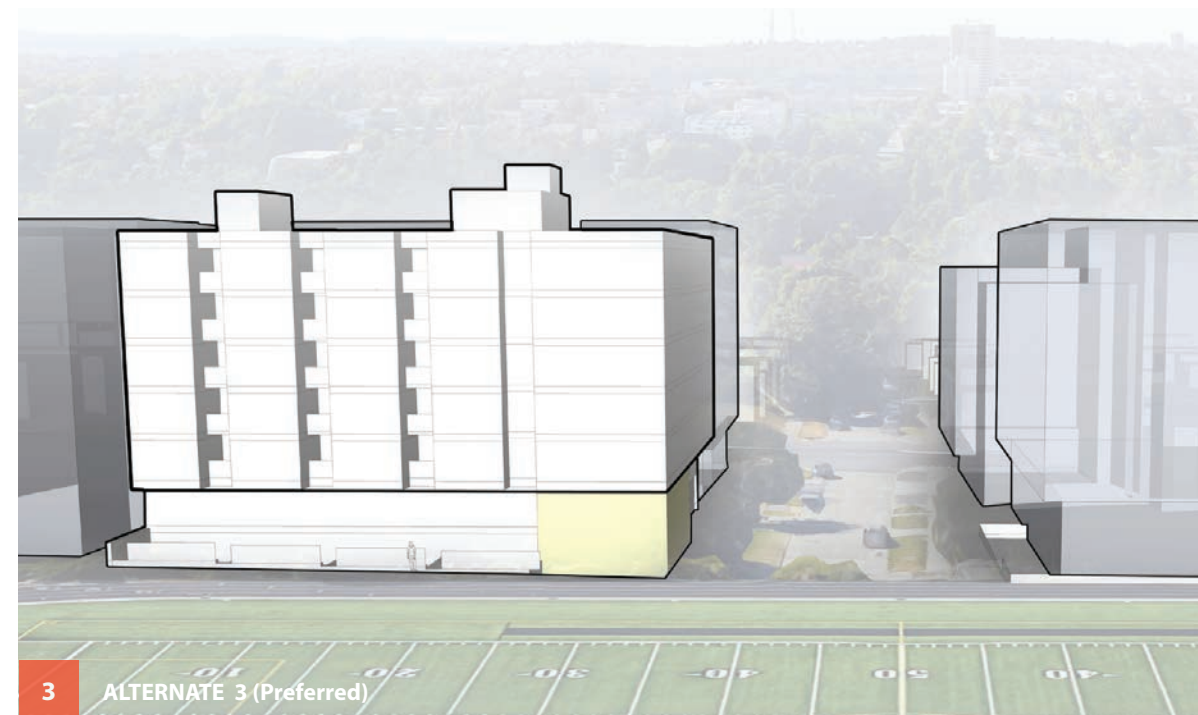
ALTERNATE 3 (Preferred)
Levels 3 - 7



ALTERNATE 3 (Preferred)
Roof Level



2 **ALTERNATE 3 (Preferred)**

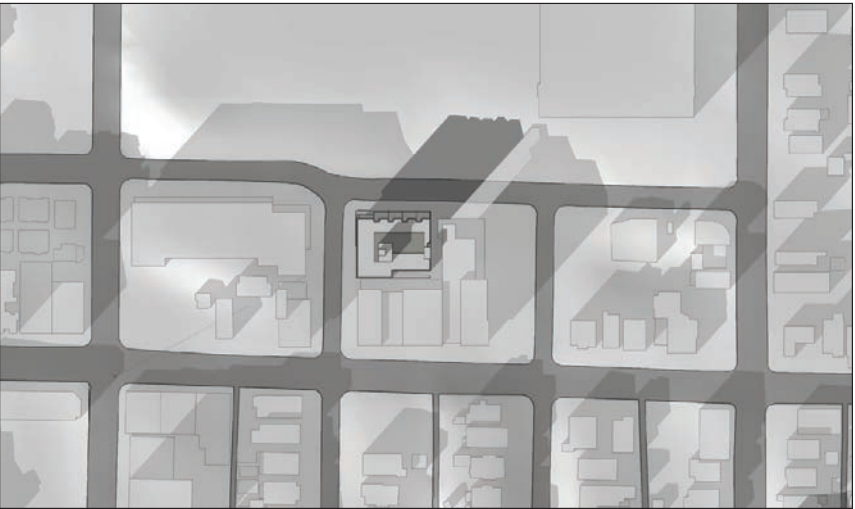
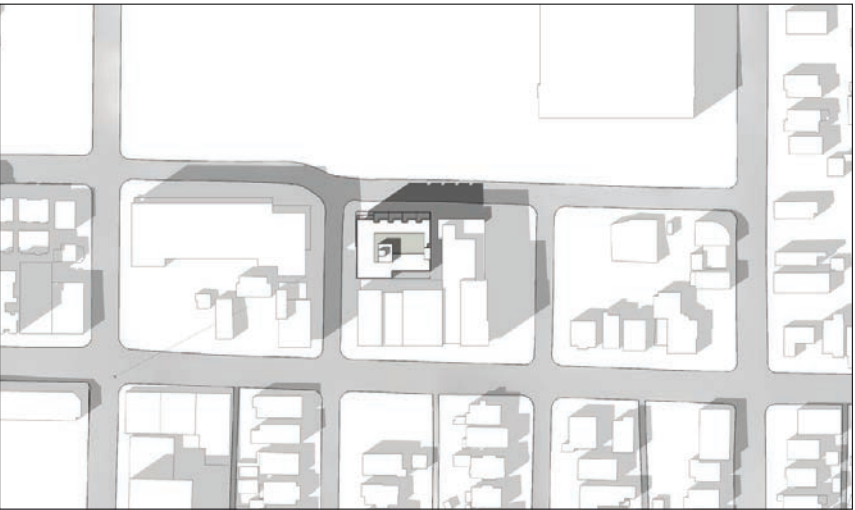
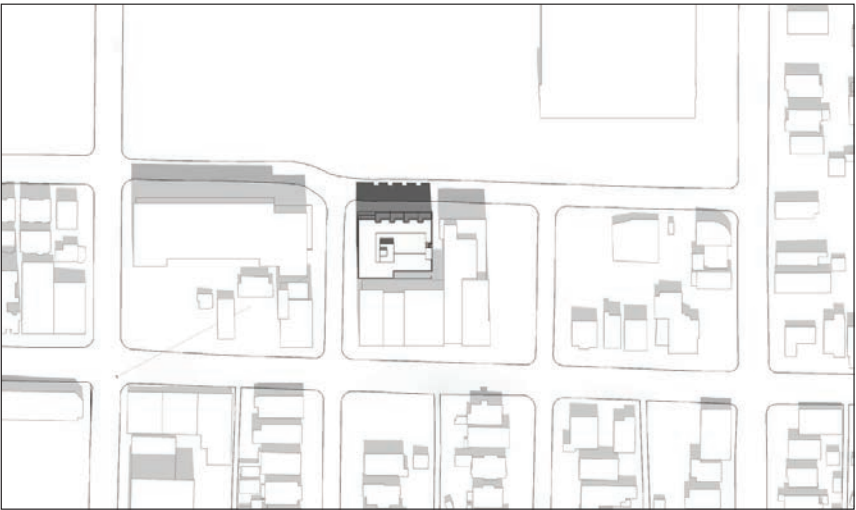
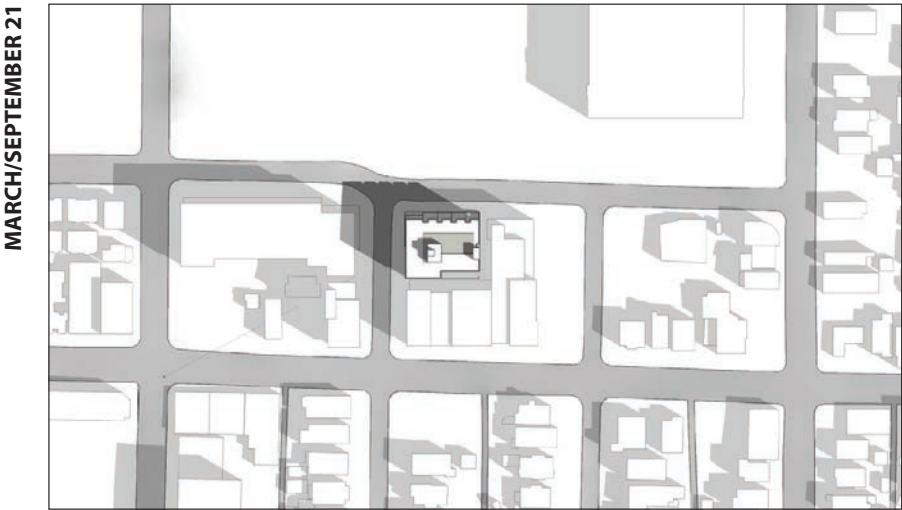
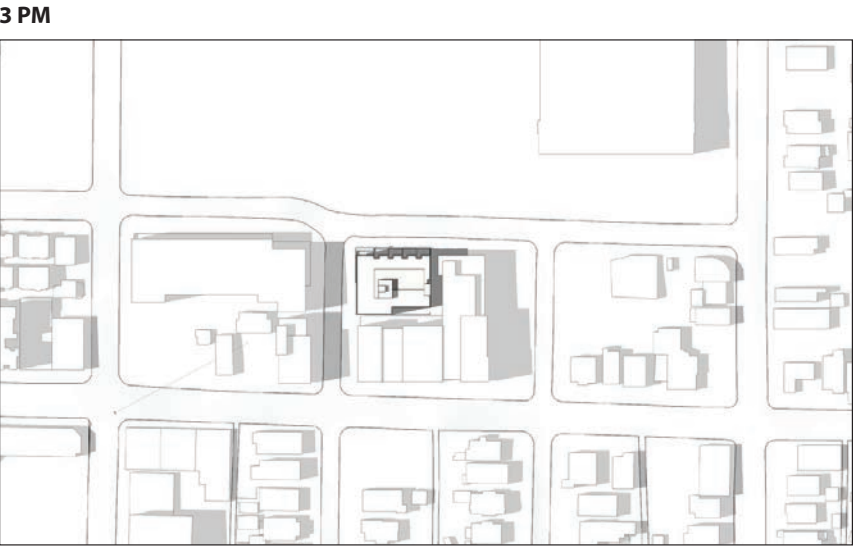
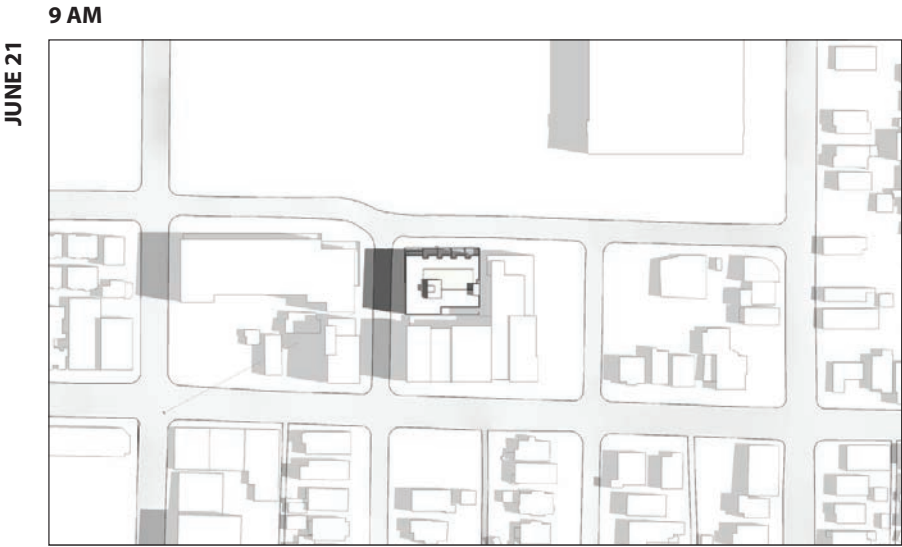


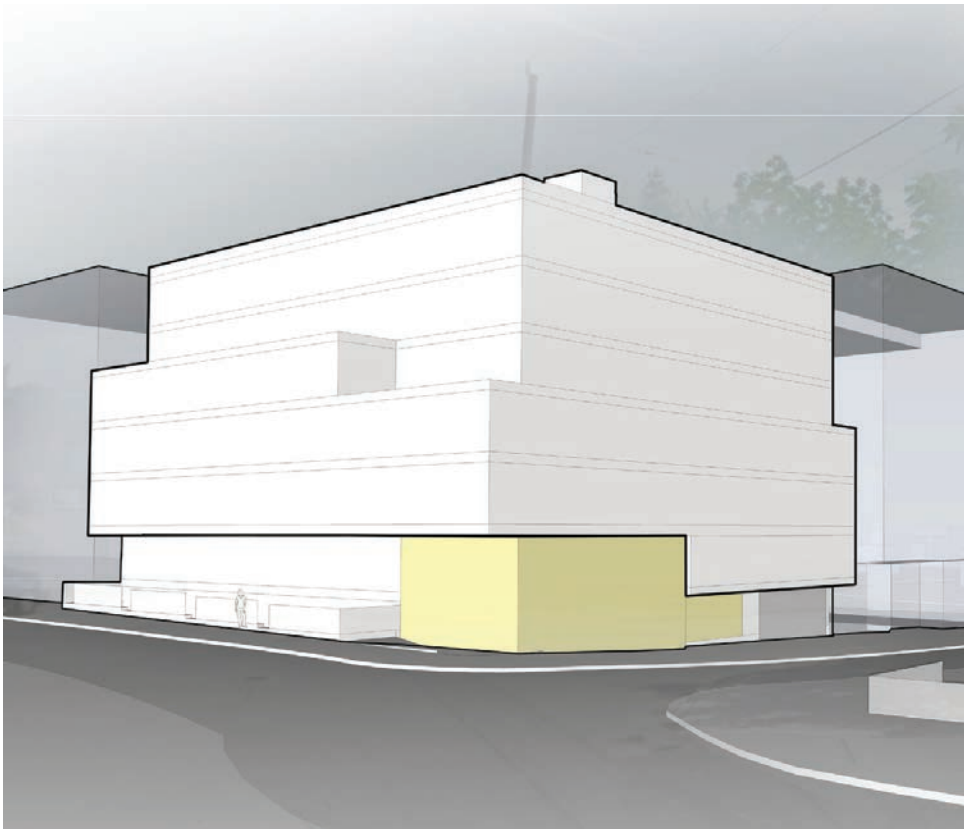
3 **ALTERNATE 3 (Preferred)**



4 **ALTERNATE 3 (Preferred)**

PREFERRED ALTERNATIVE SHADOW STUDY





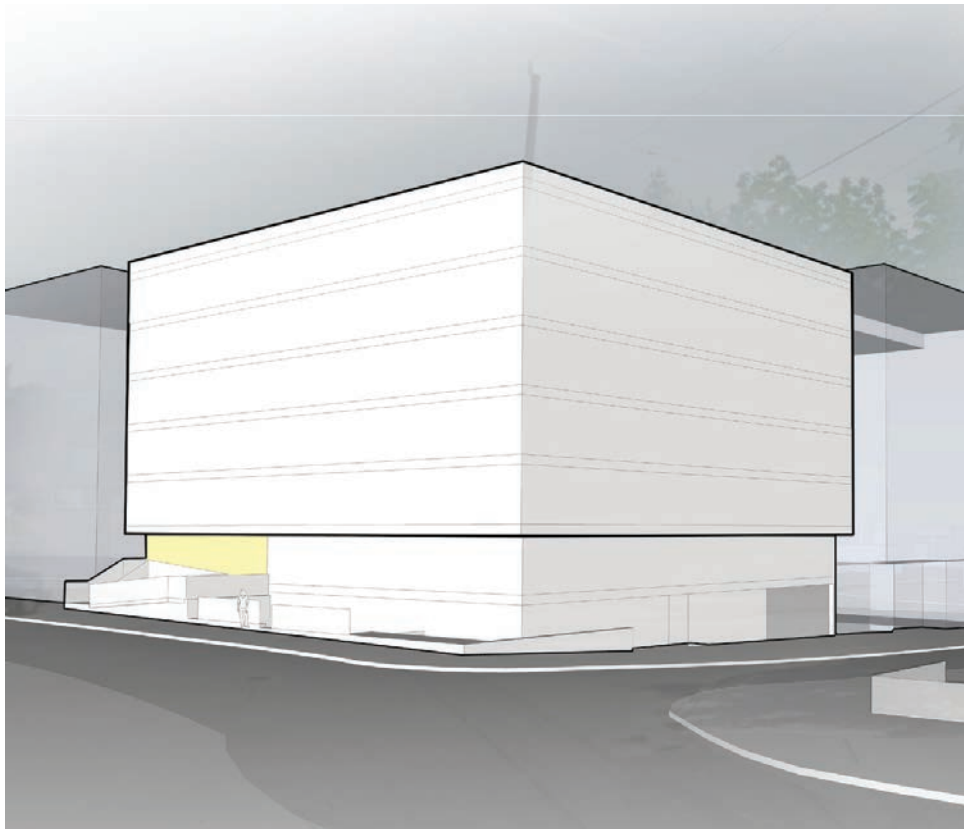
ALTERNATE 1 (NO DEPARTURES)

Summary

Stories: 7 (5-over-2 + 2 below grade)
Unit Count: 71
Floor Area: 55,750-sf Residential
19,800-sf Parking
Parking: 38 stalls

Potential Departures (Administrative)

- [SMC 23.54.030.D3](#) Driveway slope



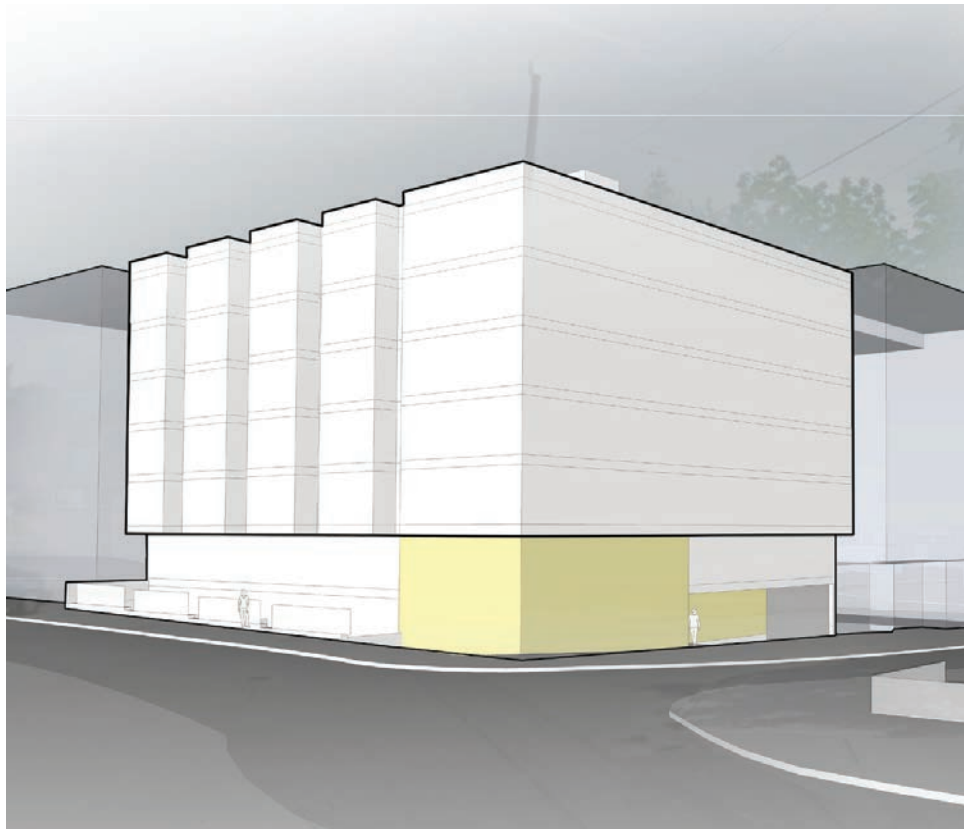
ALTERNATE 2

Summary

Stories: 7 (5-over-2 + 2 below grade)
Unit Count: 71
Floor Area: 54,600-sf Residential
19,800-sf Parking
Parking: 38 stalls

Potential Departures

- [SMC 23.47A.009.1a.1](#)) Setback requirements at NE 66th St
- [SMC 23.47A.008.A3](#) Street-level, street-facing facades at NE 66th St
- [SMC 23.47A.009.1a.2](#)) Setback requirements at Brooklyn Ave NE
- [SMC 23.47A.008.D2](#) Street-Level Street-Facing facades at Brooklyn Ave NE
- [SMC 23.54.030.G1](#) Sight Triangle
- [SMC 23.54.030.D3](#) Driveway slope



ALTERNATE 3 (PREFERRED)

Summary

Stories: 7 (5-over-2 + 2 below grade)
Unit Count: 71
Floor Area: 53,220-sf Residential
19,800-sf Parking
Parking: 38 stalls

Potential Departures

- [SMC 23.47A.009.1a.1](#)) Setback requirements at NE 66th St
- [SMC 23.47A.008.A3](#) Street-level, street-facing facades at NE 66th St
- [SMC 23.47A.009.1a.2](#)) Setback requirements at Brooklyn Ave NE
- [SMC 23.54.030.G1](#) Sight Triangle
- [SMC 23.54.030.D3](#) Driveway slope

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STREETSCAPE + ROOF



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N



generous planter w/ walkoff



fire and killer view



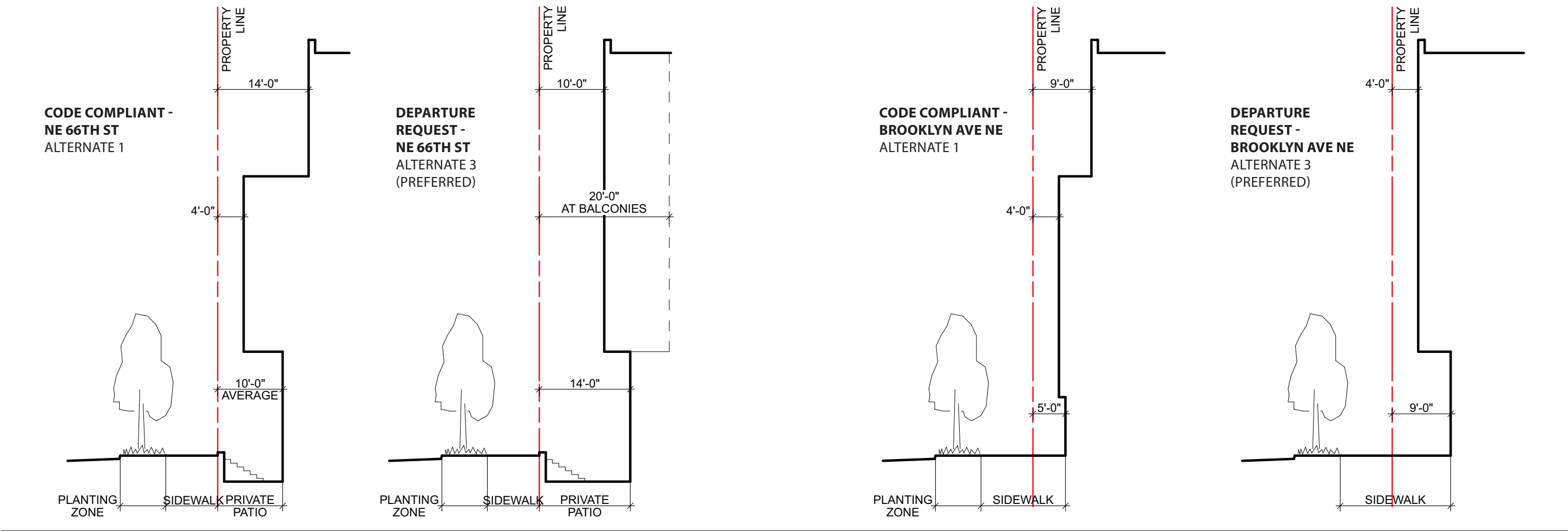
entry with stone cubes



treelets

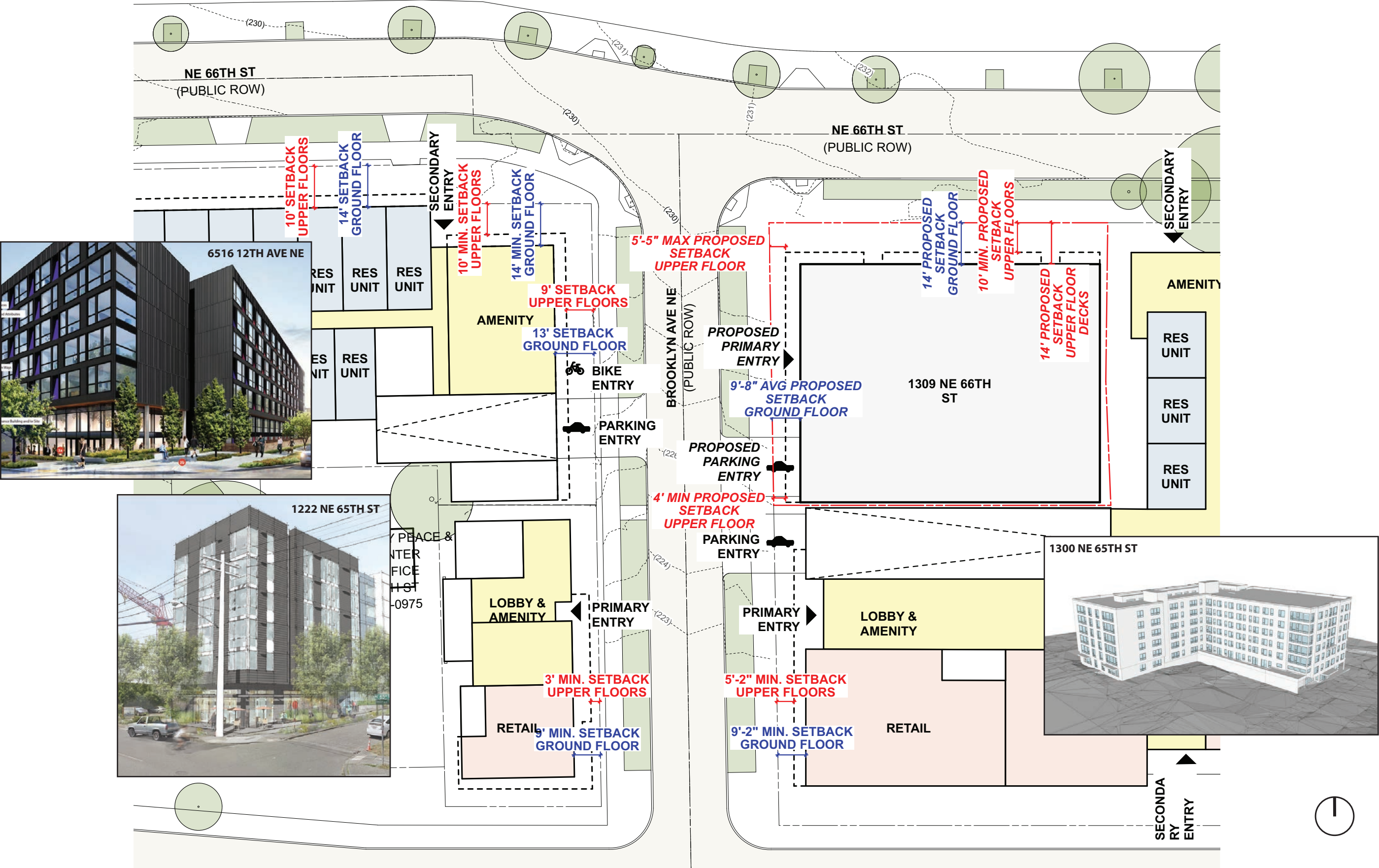
DEPARTURE REQUEST DIAGRAMS

DESIGN STANDARD	DEPARTURE REQUEST	RATIONALE FOR REQUESTED DEPARTURE	SUPPORTING DESIGN GUIDELINES
1 23.47A.009.D.a 1) Setback Requirement at Northeast 66th Street Provide an average ground level setback of 10 feet along the length of the street property line and a minimum upper level setback of 4 feet at 45 feet of height and above	Request to depart from the additional 4-foot upper level setback and instead provide an additional 4-foot setback at the street level and second level	Neighborhood Conditions: Setbacks along NE 66th St are in place for multiple purposes: provide access to light, air, and views to and from Roosevelt High School and Athletic Field, maintain view corridor west to the Olympic Mountains, and provide ample space to continue the campus-like pedestrian experience. -A project goal is to enhance the pedestrian environment along NE 66th St. Inverting the setback to ground level will provide this project with additional space for improvements, including space for street trees, pedestrian seating, and residential unit yards.	CS2-B2 Connection to the Street: Identify opportunities for the project to make a strong connection to the street. PL1-B3 Pedestrian Amenities: Opportunities for creating lively, pedestrian oriented open spaces. RDG CS3-iii Reinforce a vibrant streetscape. RDG PL2-iiii Pedestrian amenities are encouraged. RDG PL3-li High School, Green Streets, and Green Ways: Provide a more intimate, smaller-scale residential environment on blocks adjacent to the high school.
2 23.47A.008.A.3) Street-level, street-facing facades at NE 66th St Street-level street-facing facades shall be located within 10 feet of the street lot line, unless wider sidewalks, plazas, or other approved landscaped or open spaces are provided.	Request to depart from the maximum 10-foot distance to street lot line and instead provide 14-feet to the street lot line to include landscaped and open spaces.	The rationale is consistent with Departure 1.	The supporting design guidelines are consistent with Departure 1.
3 SMC 23.47A.009.D.1.a 2) Setback Requirements at Brooklyn Avenue Northeast An average ground-level setback of 5 feet along the length of the street property line and a minimum upper-level setback of 4 feet. The minimum upper-level setback shall be provided in addition to the required ground-level setback at all points along the length of the street property line at 45 feet of height and above, as measured from average finished grade.	Request to depart from the additional 4-foot upper level setback and instead provide an additional 4-foot setback at the street level and second level.	Neighborhood Conditions: Setbacks along Brooklyn Ave NE are in place to maintain access to light, air, and views to and from Roosevelt High School and Athletic Field. - A project goal is to enhance the pedestrian environment along Brooklyn Ave NE. Inverting the setback to ground level will provide this project with additional space for improvements, including a wider sidewalk, space for street trees, and pedestrian seating. Additionally, the request is in keeping with departures sought by and granted to adjacent developments on Brooklyn, promoting consistent massing and a distinct neighborhood character.	DC2-A1 Site Characteristics and Uses: Arrange the mass of the building taking into consideration the characteristics of the site. DC2-B1 Façade Composition: Design all building facades.



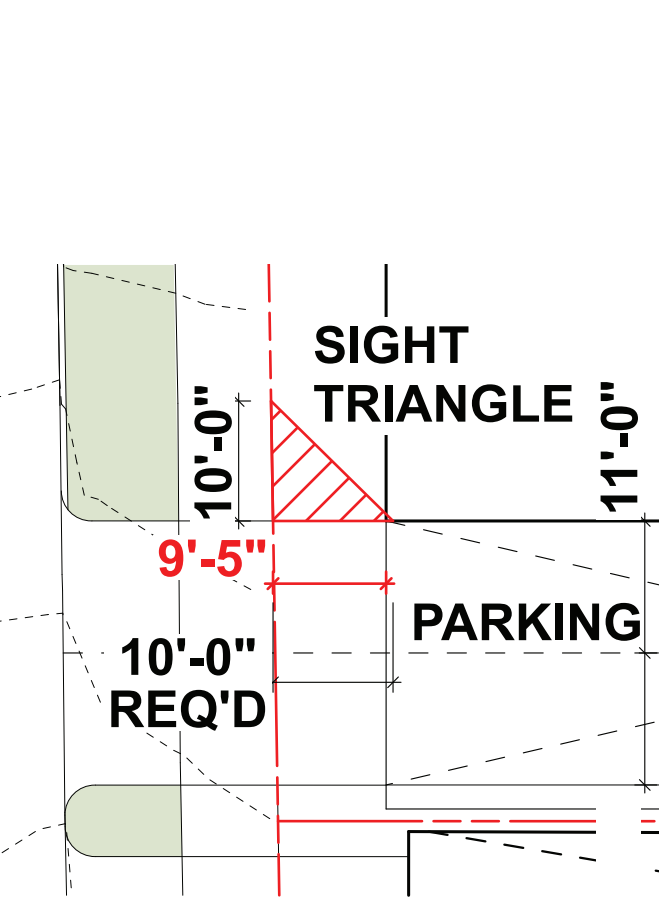
DESIGN STANDARD	DEPARTURE REQUEST
<div>1</div> <div>23.47A.009.D.a 1) Setback Requirement at Northeast 66th Street</div> <div>An average ground-level setback of 10 feet along the length of the street property line and a minimum upper-level setback of 4 feet. The minimum upper-level setback shall be provided in addition to the required ground-level setback at all points along the length of the street property line at 45 feet of height and above, as measured from average finished grade.</div>	<div>Request to depart from the additional 4-foot upper level setback and instead provide an additional 4-foot setback at the street level and second level.</div>
<div>2</div> <div>23.47A.008.A.3) Street-level, street-facing facades at NE 66th St</div> <div>Street-level street-facing facades shall be located within 10 feet of the street lot line, unless wider sidewalks, plazas, or other approved landscaped or open spaces are provided.</div>	<div>Request to depart from the maximum 10-foot distance to street lot line and instead provide 14-feet to the street lot line to include landscaped and open spaces.</div>
<div>3</div> <div>SMC 23.47A.009.D.1.a 2) Setback Requirements at Brooklyn Avenue Northeast</div> <div>An average ground-level setback of 5 feet along the length of the street property line and a minimum upper-level setback of 4 feet. The minimum upper-level setback shall be provided in addition to the required ground-level setback at all points along the length of the street property line at 45 feet of height and above, as measured from average finished grade.</div>	<div>Request to depart from the additional 4-foot upper level setback and instead provide an additional 4-foot setback at the street level and second level.</div>
<div>4</div> <div>SMC 23.54.030.G.1 Sight Triangle</div> <div>For two way driveways or easements 22 feet wide or more, a sight triangle on the side of the driveway used as an exit shall be provided, and shall be kept clear of any obstruction for a distance of 10 feet from the intersection of the driveway or easement with a driveway, easement, sidewalk, or curb intersection if there is no sidewalk.</div>	<div>Allow the use of mirrors, textured pavement, and in ground warning lights in lieu of providing the full sight triangle to diminish presence of garage entry at the street</div>
<div>5</div> <div>SMC 23.54.030.D.3. Driveway Slope</div> <div>No portion of a driveway, whether located on a lot or on a right-of- way, shall exceed a slope of 15 percent, except as provided in this subsection 23.54.030.D.3. The maximum 15 percent slope shall apply in relation to both the current grade of the right-of-way to which the driveway connects, and to the proposed finished grade of the right-of-way if it is different from the current grade. The ends of a driveway shall be adjusted to accommodate an appropriate crest and sag. The Director may permit a driveway slope of more than 15 percent if it is found that:<div>a. The topography or other special characteristic of the lot makes a 15 percent maximum driveway slope infeasible;</div><div>b. The additional amount of slope permitted is the least amount necessary to accommodate the conditions of the lot; and</div><div>c. The driveway is still useable as access to the lot.</div></div>	<div>Allow a driveway slope in excess of 20%.</div>

The following Development Departures have been identified as potentially necessary to achieve the preferred design alternate. If the board indicates their willingness to consider the departures, additional study and refinement of the design will be conducted in advance of the Design Recommendation meeting.

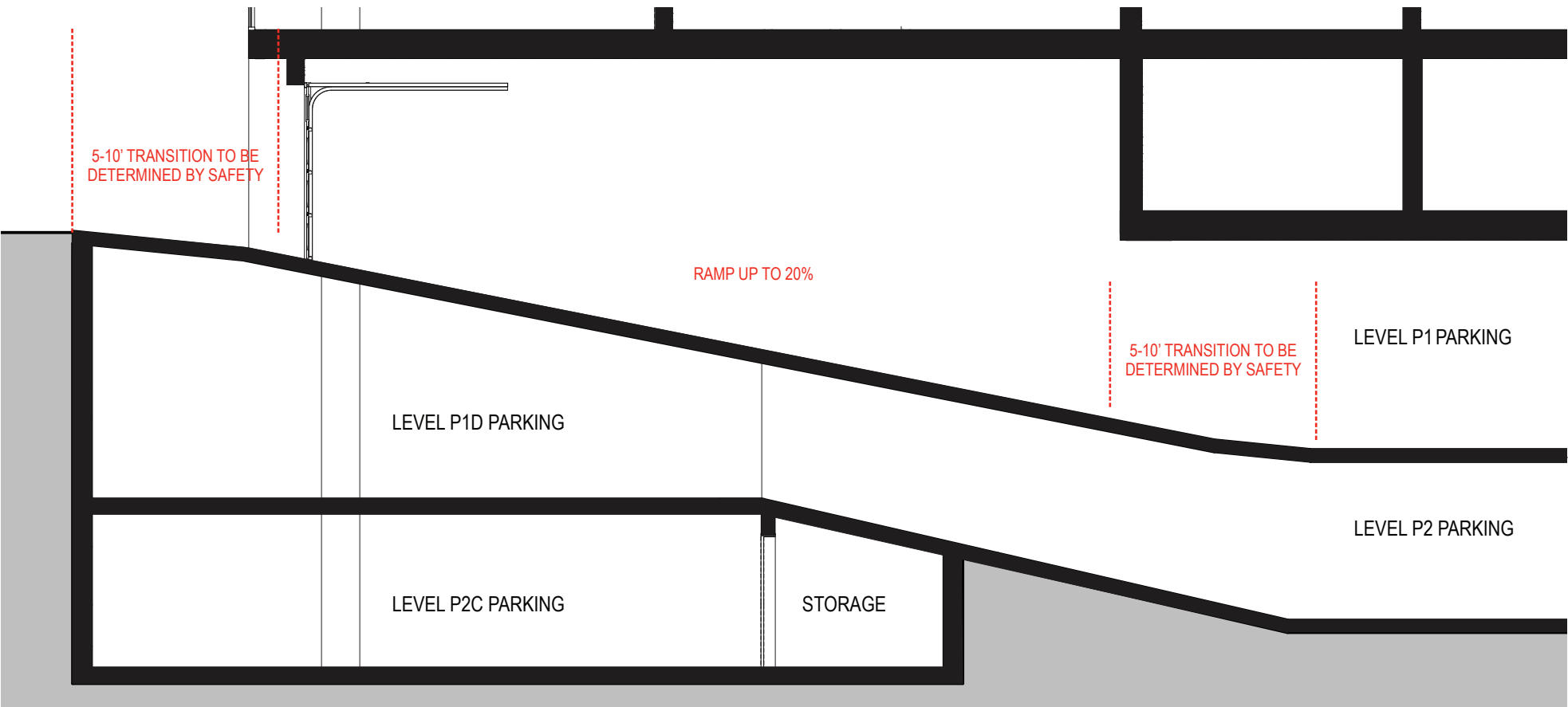


DEPARTURE REQUEST DIAGRAMS

DESIGN STANDARD	DEPARTURE REQUEST	RATIONALE FOR REQUESTED DEPARTURE	SUPPORTING DESIGN GUIDELINES
4 SMC 23.54.030.G.1 Sight Triangle For two way driveways or easements 22 feet wide or more, a sight triangle on the side of the driveway used as an exit shall be provided, and shall be kept clear of any obstruction for a distance of 10 feet from the intersection of the driveway or easement with a driveway, easement, sidewalk, or curb intersection if there is no sidewalk.	Allow the use of mirrors, textured pavement, and in ground warning lights in lieu of providing the full sight triangle to diminish presence of garage entry at the street	Neighborhood Conditions: Brooklyn Ave NE is a sidestreet that sees some increase in pedestrian traffic during school school hours. - To minimize the impact of the driveway along the street and maintain the street frontage, the sight triangles will be limited. Use of alternative measures will help maintain a safe transition.	DC1-B1.b Where driveways ... are unavoidable, minimize the width as much as possible. DC1-B.1c Employing a multi-sensory approach to areas of potential vehicle- pedestrian conflict such as garage exits/ entrances. Design features may include: warning lights and sounds, and similar safety devices.
5 SMC 23.54.030.D.3. Driveway Slope No portion of a driveway, whether located on a lot or on a right-of- way, shall exceed a slope of 15 percent, except as provided in this subsection 23.54.030.D.3. The maximum 15 percent slope shall apply in relation to both the current grade of the right-of-way to which the driveway connects, and to the proposed finished grade of the right-of-way if it is different from the current grade. The ends of a driveway shall be adjusted to accommodate an appropriate crest and sag. The Director may permit a driveway slope of more than 15 percent if it is found that: a. The topography or other special characteristic of the lot makes a 15 percent maximum driveway slope infeasible; b. The additional amount of slope permitted is the least amount necessary to accommodate the conditions of the lot; and c. The driveway is still useable as access to the lot.	Allow a driveway slope up to 20%.	Neighborhood Conditions: The Roosevelt Commercial Core and Pedestrian Overlay area is in transition away from cars and parking. - The project seeks to minimize the presence of covered parking at street level and provide all parking below grade. The driveway ramp slope is a product of required clearances and dimensions available within the constraints of the lot. The team will work with the Director to provide the minimum slope that satisfies these conditions.	N/A



SIGHT TRIANGLE DIAGRAM



DRIVEWAY SLOPE DIAGRAM

REPRESENTATIVE PROJECTS

Weinstein A+U is recognized as one of the Northwest’s leading design firms and has continually demonstrated design excellence on a broad array of projects for State, City, Federal, private, and not-for-profit clients. We are passionate about our city and the shaping of its urban neighborhoods through the integration of architecture and urban design is central to our practice.

Well-designed and thoughtful urban housing is a special concern of ours, and we have worked aggressively to advance the expectations of mixed-use projects in Seattle, both technically and aesthetically. While each project presents very specific challenges, a number of recurring themes inform much of our work and form the basis of our approach to housing design:

- All of our buildings are situational and are inseparable from their sites. They sit comfortably within their established neighborhoods, drawing from established precedents while looking to the future
- Well-designed unit plans are essential to the success of a housing project. While the functionality of each unit type is important, the organization of units across a floor plate and their influence on building elevations is equally important
- Appropriately located and proportioned open space is a significant design determinant for most mixed-use and urban housing projects
- We avoid arbitrary façade embellishment. Instead, we utilize the organization of individual units and their aggregation to establish the pattern and rhythm of multi-family facades that is further informed by site organization and orientation.
- The constrained budgets for typical mixed-use projects demand careful consideration of a project’s primary orientation and configuration to provide cost effective sustainable design strategies
- The scale and proportion of new mixed-use buildings must address, but need not directly reflect, those of adjacent structures. Plan, section, and elevation strategies should be integrated to provide a comprehensible “read” of the building’s composition and organization



- 1 Agnes Lofts, 1433 12th Avenue
- 2 19th and Mercer Mixed-Use Building, 526 19th Avenue E
- 3 2026 E Madison Mixed-Use Building, (unbuilt)
- 4 The Rooster Mixed-Use Building, 900 NE 65th Street (under construction)
- 5 Ventana at the Market, 2100 Western Ave
- 6 SCCA Patient House, 207 Pontius Ave N
- 7 Compass Center Housing, 1753 NW 56th Street
- 8 Belroy Apartments, 703 Bellevue Ave E
- 9 Banner Building, 2600 Western Avenue